

WARD: Lawrence Hill **CONTACT OFFICER:** Katy Dryden
SITE ADDRESS: 25 Redcliff Street 14/14A St Thomas Street Bristol BS1 6LR

APPLICATION NO: 14/05459/F Full Planning
EXPIRY DATE: 3 February 2015

Demolition of buildings on site, retention of 14 St Thomas Street façade and redevelopment to provide up to 128 no. residential dwellings (C3 Use Class), 339m² retail floor space (A1 & A3 Use Class) with associated works.

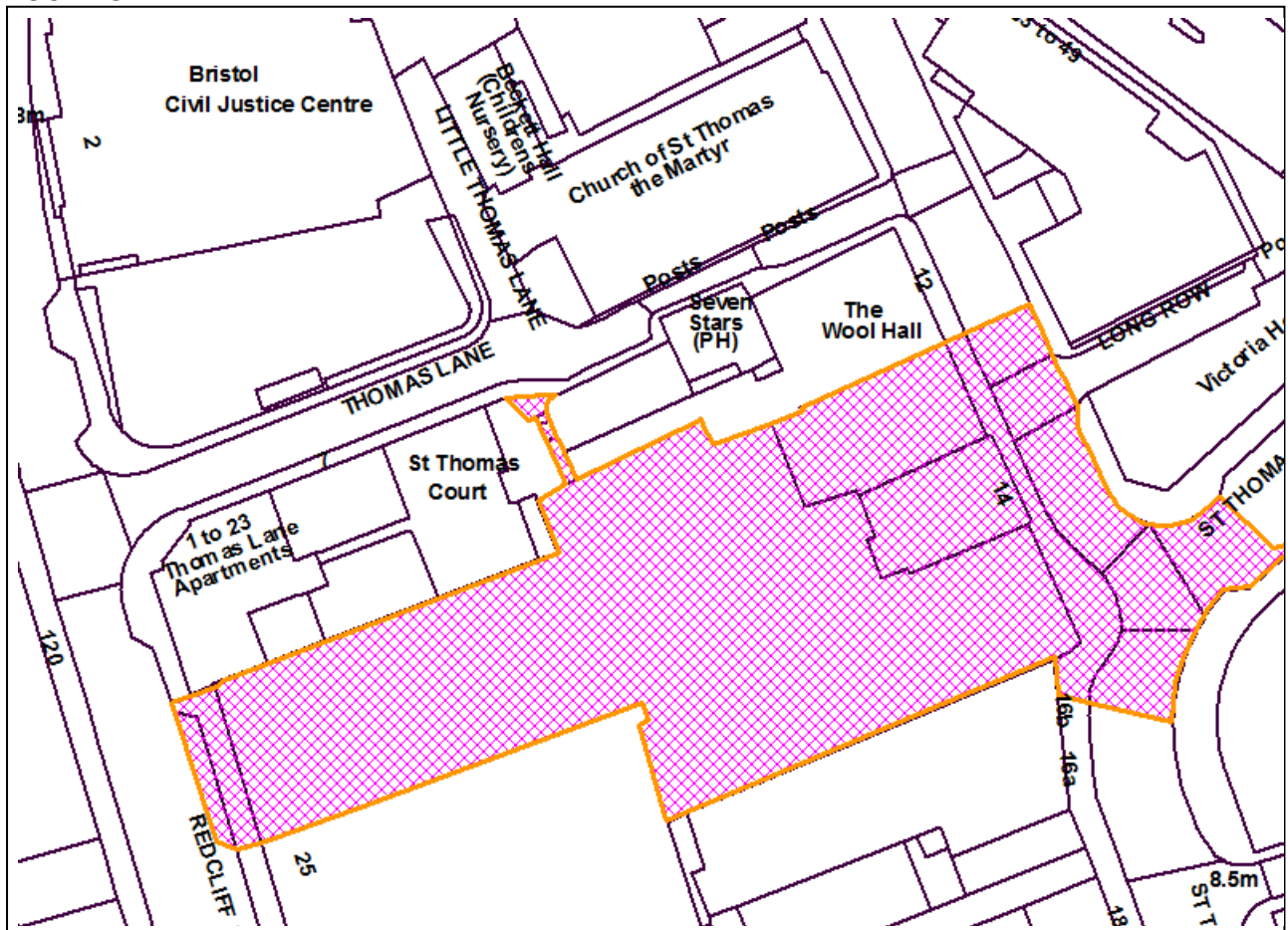
RECOMMENDATION: GRANT subject to Planning Agreement

AGENT: GVA Grimley Ltd
St Catherine's Court
Berkeley Place
Bristol
BS8 1BQ

APPLICANT: Redcliff MCC LLP
C/o Agent

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



Development Control Committee B – 29 April 2015**Application No. 14/05459/F: 25 Redcliff Street 14/14A St Thomas Street Bristol BS1 6LR****BACKGROUND**

Planning permission is sought to redevelop a significant site in North Redcliffe which comprises of long-term derelict, former warehousing buildings. The development would entail the demolition of the existing buildings on the site, retention of 14 St Thomas Street façade and redevelopment to provide 128 no. residential dwellings (C3 Use Class), 339m² retail floor space (A1 & A3 Use Class) with associated works.

The application was received in November 2014 and has been subject to revisions to address design concerns, principally relating to the St Thomas Street elevation and in response to concerns regarding the 'liveability' and internal arrangement of the residential blocks sited within the development.

Revisions have also been secured to increase renewable energy generation thereby reducing carbon dioxide emissions from residual energy use in the buildings. The revised application has been subject to a further round of public consultation.

The application submission has also been subject to an affordable housing viability appraisal where external consultants have been employed to assess the viability report submitted with the application. The findings of the report have been agreed and it is recommended that £240,000 towards off-site provision of affordable housing is secured through a S106 agreement.

A Planning Performance Agreement (PPA) has been submitted relating to the site to the south of the current application submission. The PPA sets out a programme for delivering a planning pre application enquiry in July 2015 relating to a mixed use scheme for 'A full planning application for redevelopment of former industrial buildings Redcliff Street/Three Queens Lane/St Thomas Street Redcliffe Bristol for residential led, high density mixed use development, landscaping and essential infrastructure'.

SITE DESCRIPTION

The site falls within the Redcliffe neighbourhood. This is reflected by the sites inclusion in the 'Redcliffe' chapter of the Bristol Central Area Plan (BCAP) and its inclusion within the Redcliffe Neighbourhood Planning Area.

The Redcliffe area contains a mixture of land uses, which has evolved through the areas' historic links with the city docks, post war rebuilding and major highway construction. At the centre of Redcliffe is the medieval church of St Mary Redcliffe.

The site is located within the Redcliffe Conservation Area and No.14 St Thomas Street is designated as an unlisted building of merit within the Redcliffe Character Appraisal.

To the immediate north of the application site sit two listed buildings in the form of the Wool Hall and Fleece Public House and the Seven Stars Public House. Both are Grade II listed buildings, and both sit outside of the red line site application boundary. Further north of Thomas Lane sits the Grade II* listed Church of St Thomas.

The application site forms part of a larger parcel of land contained within the North Redcliffe area and which was the subject of a master plan associated to an earlier outline planning application, also known as Redcliffe Village. The application site (Plot 7 on the above mentioned master plan) is at the northern most end of the Development framework area and encompasses a collection of warehouse buildings that span the full width of the application site with frontages on both Redcliff and St Thomas Street.

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The site falls within Flood Zone 2, along with a significant portion of Redcliffe. This places it at a medium risk of flooding.

RELEVANT PLANNING HISTORY

02/01862/P: Outline application for the erection of new buildings for mixed use development comprising residential (Class C3), serviced apartments (Sui Generis), office (Class B1), workspace (Class B1), retail (Class A1), restaurant/bar uses (Class A3), crèche (Class D1), doctors' surgery (Class D1) and leisure space (Class D2) together with ancillary access and servicing arrangements. GRANTED 20 May 2005

02/01864/LC: Demolition of existing buildings. GRANTED 17 October 2005

02/04697/F: Redevelopment of former warehouse and office building to provide 2 no. office buildings, with a bridge link at two levels and basement car parking. REFUSED 21 March 2003

02/01864/LC: Demolition of existing buildings. GRANTED 17 October 2005

03/02421/F: Redevelopment of former warehouse and office building to provide 2 no. office buildings with a bridge link at two levels and basement car parking. GRANTED 15 June 2007

03/02415/LC: Demolition of 16 and partial demolition of 14 St Thomas Street. GRANTED 15 June 2007

06/05205/LC: Variation of conditions 3 & 4 of approval ref. 02/01864/LC to allow for phased demolition works and phased redevelopment works. GRANTED 15 May 2007

12/02159/R: Extension of time limit for planning permission 03/02421/F - Redevelopment of former warehouse and office building to provide 2 no. office buildings with a bridge link at two levels and basement car parking (Major application). Granted 22 March 2013

14/02305/PREAPP: This pre application submission presented a draft version of the submitted scheme. The principle of development, i.e. the demolition and redevelopment of the site for residential and retail purposes, was accepted.

EIA Screening request for current application submission - confirmed on 16 September 2014 that the scheme was not an EIA development and did not require the submission of an environmental report.

APPLICATION DETAIL

Permission is sought for the demolition of the existing buildings on the site, retention of 14 St Thomas Street façade and redevelopment to provide 128 no. residential dwellings (C3 Use Class), 339m² retail floor space (A1 & A3 Use Class) with associated works.

The application site relates specifically to existing premises stretching between 25 Redcliffe Street to the west and 14-14a St Thomas Street to the east. The last known uses comprise warehouse (B8 Use Class) and office space (B1a Use Class). 14 St Thomas Street is currently used as office space with two current occupiers, whilst 14a St Thomas Street and 25 Redcliff St are currently let as car parking for a variety of individuals and companies (approximately 130 cars). 25 Redcliff Street comprises a large, elongated warehouse building.

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At ground floor level, three separate retail units are to be provided, with one fronting onto Redcliffe Street and two fronting onto St Thomas Street.

128 residential units will be accommodated in two buildings of seven storeys in height, with the lowest accommodation provided at first floor level. 4 residential units are below the minimum residential space standards at 40sqm.

The proposed mix of residential units includes:

- 1 bed units = 61, equivalent to 48% share
- 2 bed units = 66, equivalent to 52% share
- 3 bed units = 1, equivalent to 1% share

14 of the above units are duplexes, representing a share of 11%. In addition, there are 18 dual aspect residential units.

Please note the number of residential units has been reduced from 136 residential units in the original application submission to 128 units following design alterations to the proposal.

Vehicular access to the site will be provided from St Thomas Street into a new semi-private but publicly accessible central courtyard area from which each block has a separate, secure parking area. The St Thomas Street block features 20 car parking spaces including 3 disabled bays and 1 motorcycle bay. In addition, secure cycle parking is provided for 116 cycles.

The Redcliff Street block features parking for 7 vehicles including 2 disabled parking bays and 78 cycle parking spaces. New residential entrances to the two internal blocks (secured through revisions to the scheme) can also be accessed from the courtyard area as well as further secure cycle parking for both residents and visitors. 6 parking spaces allow for electric charging.

In total, 210 cycle spaces are provided with 196 secure cycle parking spaces provided for residents and 16 spaces for visitors and the proposed retail units.

PRE APPLICATION COMMUNITY INVOLVEMENT

A Statement of Community Involvement was submitted with the application, in accordance with the Council's requirements for major schemes. Whilst requested by the City Council, a statement has not been prepared by the Bristol Neighbourhood Planning Network (BNPN) for the planning application in this instance.

RESPONSE TO PUBLICITY AND CONSULTATION

Site notices were erected and letters were sent out to adjoining owners and occupiers. Following the receipt of revised plans in March 2015 a further round of public consultation was initiated, including letters sent to all those who had been previously been consulted.

Three letters of objection have been received regarding demolition noise, dust, disruption and structural issues. Please note that whilst these issues are not planning considerations, it is recommended that a construction management plan is secured through planning condition given the scale of the development and it's relationship with adjoining land uses. A further letter of objection has been received regarding the amenity impact of the proposal which is discussed in more detail in the relevant key issue of this planning report.

Development Control Committee B – 29 April 2015**Application No. 14/05459/F: 25 Redcliff Street 14/14A St Thomas Street Bristol BS1 6LR****A neutral comment has been received from the Church of St Thomas, Thomas Lane:**

‘Although we are not objecting to the principle of development, we are concerned about the potential impact on the church. We think there could be a significant opportunity here to improve the setting of the church and opportunities to develop a local community, however building works next to a much visited and utilised tourist attraction would also need to be taken into consideration, we would welcome a meeting with the developer’.

English Heritage has commented as follows:

We have provided a pre-application response on this proposals (dated 2 July 2014) and, after checking our records, on a previous outline proposal for the site in 2008. In both responses we raised concerns regarding the scale and massing and the harmful impact that they would have on the character and appearance of the Redcliffe Conservation Area. As well as concerns regarding the impact of the proposals when viewed from the south, along both Redcliff and St Thomas Streets, there are also concerns regarding the impact of the proposals from the north. We note views from that direction do not appear to have been provided, nor has a north elevation. The proposals have the potential to be an overbearing and incongruous addition to the townscape when viewed from that direction, including across the open space in front of the Civil Justice Centre and from Little Thomas Lane. The proposals also have the potential to be harmful to the setting of the Grade II listed buildings on the corner of St Thomas Street and Thomas Lane.

No further response received to revised plans.

The Victorian Society has commented as follows:

In light of the potential damage to 14 St Thomas Street and the conservation area by the proposed works, we recommend that consent to the application is refused.

14 St. Thomas Street is an attractive, decorative late Victorian three-storey warehouse. Its vibrant red brick walls contrast with yellow bricks used as decorative features for the segmental arches, hood-moulds, door and window reveals, decorative bands and the cornice. It is a building of merit which unusually survives intact after war damage to Bristol City and makes a positive contribution to the Redcliffe Conservation Area.

We are concerned as to the retention of the original façade of 14 St. Thomas Street. It is proposed to demolish the rear and side walls of the building retaining only the façade. However, the Design and Access Statement states that to ensure the future integrity of the structure it might be necessary to dismantle and later reconstruct the original façade, which is to be retained (paragraph 3.304). 14 St. Thomas Street is one of the few characterful buildings remaining on this street and every effort should be made to retain the facade.

The demolition and subsequent rebuilding of the façade is not a satisfactory option as permanent harm would be done to the significance of the building and the conservation area. Even if the new façade were to exactly replicate the original in form, the inherent historic interest and worth of the original façade would be lost. The patina of the current façade contributes positively to the conservation area. If rebuilt, this patina would be lost and a new façade would lack historic character being rather a pastiche of the original. Instead, the façade must be conserved, sensitively repaired as necessary, rather than rebuilding a replica which would be new in appearance and lose its historical and aesthetic value.

14 St. Thomas Street makes a very positive contribution to the conservation area. Paragraph 134 of the National Policy Planning Framework (NPPF) states ‘where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset [in this case, the conservation area], this harm should be weighed against the public benefits of the proposal, including

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securing its optimum viable use'. Here, there is no public benefit for the whole of 14 St. Thomas Street to be demolished leaving only a rebuilt façade. Such development would have a harmful effect on the conservation area and a detrimental effect on the non-designated asset itself.

It is vital that the original façade is conserved and we suggest that consent only be given if the applicant guarantees that it will be retained.

In addition, we are concerned as to the scale and massing of the proposed buildings and the harmful impact they would have on the character and appearance of 14 St. Thomas Street and the conservation area. The proposed buildings are to be between seven and eight storeys high. However, 14 St. Thomas Street is three storeys high. The new buildings would dwarf the modestly-scaled historic buildings in the conservation area while, given the site's pivotal position, dominating not only the Redcliffe area but also its links to the south of Bristol with the City Centre. Therefore, the height of any new buildings should sensitively take into account those which are pre-existing.

Response to revised application submission:

'Having reviewed the revised application, we still object to the proposed scheme and refer you to our letter of 8 January 2015. As previously stated, we are concerned as to the retention of the original façade of 14 St. Thomas Street. The amended documents do not address the original statement in the Design and Access Statement that *to ensure the future integrity of the structure it might be necessary to dismantle and later reconstruct the original façade, which is to be retained* (paragraph 3.304). It is vital that the original façade is conserved and we suggest that consent only be given if the applicant guarantees that it will be retained.

Although the applicants have made some positive changes to the application such as refining the proposed elevation to St. Thomas Street and proposing finer detailing for the lower levels of the elevations, we are still concerned as to the scale of the proposed buildings and the harmful impact they would have on the character and appearance of 14 St. Thomas Street and the Redcliffe Conservation Area. The proposed buildings would be between seven and eight storeys high. The new buildings would dwarf the modestly-scaled historic buildings in the conservation area which are three to four storeys high and, given the site's pivotal position, would dominate not only the Redcliffe area but also its links to the south of Bristol with the City Centre.

In light of the above, we still recommend that this application is refused'.

The Environment Agency has commented as follows:

No objection to the proposed development subject to conditions being included in any planning permission granted. (Please see recommended conditions at the end of the report).

Bristol Civic Society has commented as follows:

'Bristol Civic Society made a detailed response to 14/02305/PREAPP and asks that the comments below are considered in conjunction with that. The Society remains content with proposed demolitions and welcomes the retention of 14 St Thomas Street in the development. Likewise the Society has no issues with the land uses. We remain of the view that the site would be better redeveloped in conjunction with the adjoining Carlisle site. However, we have considerable concerns relating to the height and mass of the proposed buildings. In particular, the elevation to St Thomas Street shows buildings exceeding the height of No14 and the Wool Hall. The height of neither of these buildings should be exceeded in order to protect their significance and preserve and enhance the character of the Conservation Area. We are concerned that the height of the central blocks will shade many of the apartments and adversely affect the amenity of the inner courtyards. It is also not clear what the impact of the taller central buildings would be on the tower of St Thomas the Martyr when viewed from the south. Sections indicate that they would diminish its significance and that would be unacceptable.

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The Society welcomes the replacement of the wooden panels originally shown in buildings fronting St Thomas Street. The proposed redevelopment contains a major residential element. The Society urges the Council to ensure that the mix of unit sizes and the proportion of affordable units complies with Council policies’.

Comments in response to revised plans:

‘Following a further meeting with the developer when we were shown new plans the Society withdrew its objection to the modelling of the St Thomas Street elevation:

The Society welcomes the proposal to demolish and redevelop buildings of little architectural quality that do not contribute to the area’s vitality and to retain 14 St. Thomas Street, an unlisted 19th century warehouse of merit. The Society supports the change of use of the upper floors to residential accommodation. The location in Redcliffe, gives an opportunity to construct the best possible residential accommodation. The Society supports the limited but realistic proposals for street level commercial use. The Society no longer objects to the impact of the scheme on the streetscape of the St. Thomas Street, which it made pre-application. The Society has seen revised drawings of this elevation that sets back the higher floors. The Society remains concerned about the proposed height of seven floors in the centre of the scheme and the degree of shadowing of the apartments that line the internal courts. The Council has established planning policy that it will apply to this aspect. The scheme will improve the public realm. The Society welcomes the planning gain of a new footpath from Redcliffe Street to Saint Thomas Street and the conservation gain that the repair and renewal the northern part of St. Thomas Street with traditional setts will bring’.

The Bristol Tree Forum has commented as follows:

‘The block plan is showing no planting in the car park areas at all despite the landscape assessment referring to BCS9 and BCAP25. I appreciate the car park is away from the public realm but the reasons behind BCS9 para 3 and BCAP25 are to improve the environmental conditions in and around the development for reasons of SUDS, air pollution urban heat island etc. Plant some trees please.’

Urban Design has commented as follows:

Comments incorporated into Key Issue C of the main report.

Archaeology Team has commented as follows:

Comments incorporated into Key Issue D of the main report.

Nature Conservation has commented as follows:

Comments incorporated into Key Issue J of the main report.

Transport Planning has commented as follows:

Comments incorporated into Key Issue F of the main report.

Pollution Control has commented as follows:

Comments incorporated into Key Issue B of the main report below.

Sustainable Cities Team

Comments incorporated into Key Issue E of the main report.

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I have reviewed the air quality assessment and can confirm that I do not have any concerns with regards to air quality. An assessment has been carried out into the potential impact from demolition and construction dusts. To mitigate these impacts measures to be contained within a CEMP have been outlined and their inclusion within a CEPM should be conditioned.

In the air quality assessment, commitment has been made to provide 1 electric vehicle charge point per 5 parking spaces as per the Parking Standards Schedule in appendix 2 of the Site Allocations and Development Management Policies which for C3 Residential use states:

For schemes where ten or more car parking spaces are proposed, one electric vehicle charging point should be provided for every five spaces. For individual dwellings with a driveway or garage, passive provision of an electric vehicle charging point should be made so that a charging point can be added in the future. (Please see Transport Key Issue F where this has been considered and included).

Contaminated Land Environmental Protection has commented as follows:-**Comments incorporated into Key Issue H of the main report.****Crime Reduction Unit** has commented as follows:-

I recently viewed details of the above development. Having looked at the plans I have no concerns or objections as leaving large properties vacant/in need of repair for prolonged periods of time only will inflame the ASB problems and give the wrong persona about this area. Redcliff has long called for more development and higher density to help bring more life and vibrancy to this area creating a place where people will want to live, additionally assist towards a sustainable city.

This proposed development will provide a predominantly residential scheme comprising of 136 dwellings across two blocks with four ground floor retail units on this site, creating a much needed, residential family homes to this area.

After Reading through the DAS statement my only comment would be the applicant applies for SbD. Secured by Design is a minimum standard for security, the applicant should familiarise themselves with this site www.securedbydesign.com to obtain site specific information. (Please note this has been recommended for inclusion as an advice).

Flood Risk Manager has commented as follows:-

The outline drainage plans and calculations are sound and we support the use of the SuDS techniques mentioned. The detailed design will need to include maintenance schedules for these features. (Please note this has been recommended for inclusion as a condition for further details).

RELEVANT POLICIES

National Planning Policy Framework – March 2012**Bristol Core Strategy (Adopted June 2011)**

BCS5	Housing Provision
BCS2	Bristol City Centre
BCS8	Delivering a Thriving Economy
BCS10	Transport and Access Improvements
BCS11	Infrastructure and Developer Contributions
BCS13	Climate Change

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BCS14	Sustainable Energy
BCS15	Sustainable Design and Construction
BCS16	Flood Risk and Water Management
BCS21	Quality Urban Design
BCS22	Conservation and the Historic Environment
BCS23	Pollution
BCS18	Housing Type

Bristol Site Allocations and Development Management Policies (Adopted July 2014)

DM1	Presumption in favour of sustainable development
DM3	Affordable housing provision: smaller sites
DM13	Development proposals on principal industrial and warehousing areas
DM14	The health impacts of development
DM15	Green infrastructure provision
DM19	Development and nature conservation
DM23	Transport development management
DM26	Local character and distinctiveness
DM27	Layout and form
DM28	Public realm
DM29	Design of new buildings
DM31	Heritage assets
DM32	Recycling and refuse provision in new development
DM33	Pollution control, air quality and water quality
DM34	Contaminated land
DM35	Noise mitigation

Bristol Central Area Plan (Adopted March 2015)

BCAP1	Mixed-use development in Bristol City Centre
BCAP2	New homes through efficient use of land
BCAP3	Family sized homes
BCAP5	Development and flood risk
BCAP6	Delivery of employment space
BCA15	Small scale retail developments and other related uses in Bristol City Centre
BCAP20	Sustainable design standards
BCAP21	Connection to heat networks
BCAP25	Green infrastructure in city centre development
BCAP29	Car and cycle parking
BCAP30	Pedestrian routes
BCAP31	Active ground floor uses and active frontages in Bristol City Centre
BCAP47	The approach to Redcliffe
BCAP SA 6	Site allocations in Redcliffe

Supplementary Planning Documents

SPD3	Future of Redcliffe (July 2006)
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Supplementary Planning Guidance

Redcliffe Conservation Area Character Appraisal

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KEY ISSUES

(A) IS THE PRINCIPLE OF THE DEVELOPMENT ACCEPTABLE?

Policy BCS2 of the Bristol Core Strategy relates to the city centre and states that Bristol City Council's role as a regional focus will be promoted and strengthened. Development will include mixed uses for office, residential, retail, leisure, tourism, entertainment and arts and cultural facilities. Policy BCS2 also states that continued improvement will be promoted in regeneration areas, including Redcliffe.

The Bristol Central Area Plan (BCAP), Adopted March 2015, allocates the site as a development opportunity (site allocation SA 603, BCAP Policy SA6). The site forms part of a wider development site (including land to the south of the proposal) which is listed as 'Redcliffe Village' in Policy SA6. Development considerations for the wider site include to:

- Provide safe vehicular access
- Explore the opportunities to narrow the surrounding streets, particularly St Thomas' Street and Three Queens' Lane, or otherwise remodel and enhance them to include wider footways, trees and informal pedestrian crossing facilities;
- Take account of the Redcliffe conservation area;
- Provide street trees, particularly where streets are not proposed to be narrowed;
- Be supported by a flood risk assessment.

Regard will be had to the additional considerations set out in SPD3 in considering any proposals for the site'.

SPD3 'The Future of Redcliffe' is relevant to the application where the site forms part of Central North Redcliffe (or Redcliffe Village) where aspirations include:

- 'Improve permeability of the central block by creating pedestrian way and public spaces through the centre.
- Narrow St Thomas Street by bringing forward the building lines.
- Provide active ground floor uses.
- Street trees should be planted on Redcliffe Street'.

The Redcliffe Development Forum are currently preparing a document which is yet to go out to public consultation. As such, the document is at an early stage and is not considered to be a material consideration for this planning application.

There are a number of previous applications which are of relevance to this application, including outline application (ref. 02/01862/P) which sought the comprehensive redevelopment of a larger site (encompassing the current site) for a mixed use scheme. This scheme included residential and retail uses (A1 and A3) and was granted in 2005 but not implemented. Whilst the site has an extant (live) permission for office development and associated parking (ref. 12/02159/R), the principle of a mixed use development on this site incorporating residential and retail uses has been demonstrated through the aforementioned mixed use scheme. The loss of the long term vacant warehousing and office provision at the site is considered to be acceptable when balanced against the overall regeneration benefits of the proposal for the Redcliffe quarter of a mixed use residential and retail development.

Is the Principle of Small scale retail development acceptable in this location?

Building upon the general retail strategy expressed within BCAP policy 15 (small scale retail development in Bristol City Centre) this policy expresses general support for the development of small scale retail, food and drink uses outside of the designated primary shopping areas and secondary frontages, with the stipulation that these should contribute positively to the vitality of the area. In terms

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of the size of the A1 and A3 uses, a scale of up to 200sq.m is considered to be acceptable. At a scale of 140m², 96m², 105m², the three proposed retail units fall within this threshold and are considered to be acceptable under this policy.

(B) ARE THE PROPOSED RESIDENTIAL UNITS ACCEPTABLE?

Policy BCS21 of the Bristol Core Strategy seeks to ensure that the amenity of existing development is safeguarded and to create a high quality environment for future occupiers.

Space standards and proposed residential mix:

The proposal is considered to satisfy the requirements of the 2011 Bristol City Council Residential Space Standards, with the proposed scheme having a range of unit sizes from 40sqm to 110sqm. Whilst 4 units measure 40sqm, which is below the recommended requirement of 45sqm for one bedroom units, given the quantum of residential development that is required in this high density development and where 124 units are in compliance with the standard, the proposal is 'on balance' considered to be acceptable.

The creation of flatted accommodation is accepted in this high density city centre location. The provision of smaller units of accommodation, including largely one and two bedroom units, is a reflection of ensuring the viability of the scheme. This mix of units is considered to be acceptable, including the provision of 1 no. three bedroom apartments suitable for family accommodation in accordance with BCS18 of the adopted Bristol Core Strategy (2011) and Policy HE3 of the emerging Bristol Central Area Action Plan (2012). Furthermore Policy BCS2 of the Bristol Core Strategy sets out a need for 7,400 new homes in the city centre over the Core Strategy period up to 2026 where the proposal will create 128 additional residential units.

Future Residential Occupier Amenity:

The scheme has been amended in order to respond to officer concerns regarding 'the liveability' of the proposal, the main changes relate to the remodelling of the ground floor area to provide a second core to each of the blocks and the creation of an internal courtyard/public space. This has reduced the length of the access corridors within the development and introduced more light into these areas through the stairwells of the cores and fully glazed ground floor entrances. With regards to natural light to the corridors this will be provided through the stairwell of the cores as there are constraints on window locations to the north and south of the blocks. The corridor lengths are reduced from the previous submission and in particular this is within the St Thomas street block where natural light has been included.

18 dual aspect units have been provided to the southern ends of both the St Thomas and Redcliff Street blocks. However, it has not been possible to incorporate dual aspect units in other areas of the blocks. A pure dual aspect scheme that provided fronts and backs does not provide a viable scheme, it is understood that this was developed and discussed with the applicant and this scheme offered approximately a 45% reduction in units, which would be undeliverable and unviable.

Concerns regarding 'the liveability' of the scheme are now considered to be satisfactorily addressed, where the revised BRE study for the revised proposal has resulted in a reduction in the number of residential units which do not achieve the BRE recommended level of at least 1.5% Average Daylight Factor (ADF) in respect of living rooms. Only 7 living rooms of the 128 proposed units achieve less than 1.5% ADF compared to the previous figure of 11. Furthermore, 2 of these 7 are only marginally below the recommended level, both at 1.46% ADF. As such, it is considered that the scheme ensures an adequate level of amenity will be enjoyed by future residents across the development and the results of the analysis are in keeping with a high density urban location.

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It is noted that the distances between the residential blocks are close (internal elevation to elevation distances) at 15.2m, 13.5 and 13m, however given the central location of the site, it is not considered that overlooking between the proposed residential units would warrant refusal of the application on these grounds. The design of the courtyard areas has been improved, including the integration of a central public courtyard space at ground floor level, in an area which was previously allocated as car parking in the original submission. Private amenity space provided in the raised courtyard areas is considered to be acceptable with public open space available within walking distance to Queens Square and Castle Park.

Adjoining Residential Amenity:

Following revisions to the scheme, the proposal now follows the form of the earlier extant consent for office development at the site (ref. 12/02159/R) with regards to the building form at the north elevation of the site. The bulk/ height and massing of the development will have an impact on adjoining residential occupiers immediately adjoining the site, to the north of the proposal at Redcliff Street (St Thomas Lane apartments). With regard to the shadowing impact of the revised scheme on the adjacent St Thomas Lane apartments, the analysis has confirmed that only five of the rear facing rooms will be noticeably affected by shadowing from the new development, experiencing a greater than 20% reduction in existing light levels. The rooms are located on the first, second and third floor of the existing residential block. Whilst it is noted that there will be an impact with regards to overbearing and loss of daylight/ sunlight to St Thomas Lane apartments, the proposal follows the height which has been previously agreed at the site and could be built out under the extant (live) consent. As such, a refusal on this basis could not be justified.

An objection has been received from the Seven Stars public house who have expressed concern regarding the proposed height of the building having a direct impact on the daylight received to the rear of the building. 'This will have an impact not only on the residential top floor, but also into the business premises first floor kitchen, stairwell and large roof light'.

Again, it should be noted that the proposal follows the form of the extant consent and has been reduced in height following officer concerns with the original application submission.

With regards to overlooking, window openings have not been included on this elevation to remove opportunities for overlooking the residential development to the north of the site.

Noise and Disturbance:

A noise impact assessment has been prepared for this application. The site is immediately adjacent to the Fleece music venue which operates under late opening hours. Pollution Control have confirmed that the noise report recommends specifications needed in order to protect any future residents from noise from the Fleece but as the report states 'This assessment is indicative only to demonstrate the feasibility of the project. Continued design development is essential once the final construction has been selected'. Therefore, detailed planning conditions are recommended for inclusion to ensure appropriate sound insulation measures are secured.

(C) IS THE PROPOSAL ACCEPTABLE WITH REGARDS TO HERITAGE AND DESIGN ISSUES?

The National Planning Policy Framework (NPPF), 2012, states that great importance should be given to the design of the built environment. In determining applications, weight should be given to outstanding or innovative designs which help to raise the standard of design in an area. Policy BCS21 of the Bristol Core Strategy seeks to deliver high quality urban design which contributes positively to an area's character and identity, creating or reinforcing local distinctiveness. Policy BCS22 is also relevant and which seeks to safeguard or enhance heritage assets and the character and setting of areas of acknowledged importance including listed buildings and conservation areas. With regards to

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the Bristol Local Plan Site Allocations and Development Management Policies, Policy DM29 is relevant which seeks to ensure that new buildings are designed to a high standard in addition to policy DM31 which gives great weight to the conservation of heritage assets.

The proposal has been subject to extensive design negotiations and discussions from the submission of the pre application enquiry in July 2014.

The City Design Group prepared the following response to the original application submission in November 2014, setting out concerns regarding the application submission as initially submitted:

Establishing a route through the site

Whilst the principle of a high density residential development is accepted, it is concerning that the potential to facilitate an east-west public route through the site has not been effectively provided.

(Please note, following detailed discussions with the applicant/ agent, it has been agreed that opportunities will be fully explored for realising an east – west public route through the larger development to the south of this site, where a formal pre application submission is anticipated in July 2016. Given the limited size of the Redcliff Street frontage in this application submission, the development to the south will provide greater opportunity to realise a meaningful route).

Impact of the proposal along St Thomas Street

The retention of the Victorian façade, a building of merit in the conservation area is acknowledged. However, bearing in mind paras 131 to 133 of the NPPF the extent of demolition is not justified. We seek the meaningful integration of the side walls in addition to the façade and internal space as a distinct element/unit. The main entrance located here is considered positive.

St. Thomas Street displays a strong historic character including St. Thomas Church (listed GII*), the Wool Hall, Seven Stars (listed GII), and no 14 St Thomas Street within the Redcliff Conservation Area. The proposed buildings adjacent to the retained Victorian facade along St Thomas Street should be sensitively designed responding to, amongst other aspects, its height, scale, massing, proportions and detailing without necessarily resulting in a pastiche solution. Although the proposed buildings respond to the rhythm of the width of the retained elevation, they do not relate to its height and scale, rising two storeys higher along the retained Victorian façade, with an inconsistent/varied setback at upper storeys. The disposition of massing creates a built form cluttering around the retained façade, insensitive to its presence and inconsistent with the surrounding building characteristics in this part of the conservation area, harming its character and appearance.

The buildings as perceived along St Thomas Street against the setting of the adjacent listed buildings is characterised by low scaled frontage and consistent building line. In comparison to the existing frontage the proposal by virtue of its overall scale of 5-7 storeys, its excessive height, varying setbacks, disposition of massing, increases the negative characteristic at upper level which are very prominent within the public realm. Part of the building mass appears above Wool hall and alongside St Thomas Church frontage. The proposals would also harm the setting of the adjacent listed buildings.

The proposal cumulatively will cause substantial harm to the fabric of the unlisted building of merit, the character and appearance of the Conservation Area and the setting of the neighbouring Listed Buildings. Hence the proposal does not conform to Section 12 of the NPPF, Sections 66 and 72 of the Planning (LBs and CA)s Act 1990 (and its subsequent amendments) and policies BCS21, BCS22, DM26, DM27 and DM 31 of the Bristol Local Plan.

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Impact of intensity and design on the townscape

As the building steps up in scale at the rear of St Thomas Street, it may be visible in medium distant views, but also, will be visible from adjacent buildings in the vicinity within the conservation area. Hence it is essential that the upper storeys make a positive contribution to the townscape. A true representation of the appearance should be provided as elevations/views. The vents should be incorporated as a feature responding and contributing to the rhythm of the build. They currently appear ad-hoc and do not relate to the building design.

Impact of intensity on amenity

Percentage of units within the development that meet daylight levels as per BRE guidance should be provided to assess the impact on amenity of future occupiers.

Public Realm

SPD3 seeks to enhance and pedestrianize the space in front of the site along St Thomas Street and has the potential to reduce the road width by extending the building line of the west facing frontages (which we acknowledge may prove difficult). However, the road width could still be narrowed with the pedestrian environment extended. It is essential that the public realm is significantly transformed in this part of the city. This should be achieved on both Redcliff Street and St Thomas Street.

A co-ordinating Public Realm Strategy would be required to suggest a build out of pavement (where necessary), consistent tree species (where proposed), street furniture typology, lighting and paving material palette, sensitive to the historic setting of buildings. The development would then be expected to deliver the public realm including carriageway/footway adjacent to its frontage and including the northern area along St Thomas Street. The Public realm should incorporate public art as an integrated solution within the design (see below). Any solution would need to be co-ordinated with any highway works proposed. The necessity of a raised table as the mechanism for reducing traffic speed is questioned.

St. Thomas Street displays strong historic character through the architecture of the St. Thomas Church, the Fleece and its associated public realm treatment - pennant flag footways, natural stone setts for carriageway, cast iron kerbs and drainage details. The application proposals should seek to take advantage of this historic character by extending the paved elements for both footway and vehicle surfaces as far the junction with St. Thomas Street East. Alternative arrangements to the proposed speed table at this junction should be considered – road narrowing with tree planting would reduce traffic speeds and provide much needed green relief in this part of Redcliff. The transition from a historic to more contemporary paving surfaces at the junction should be sensitively handled.

The requirement for tree planting on Redcliff Street addresses the overly wide visually unengaging vista and the potential for speeding traffic. As the site frontage on Redcliff Street is of limited width and in order to fulfil the aspirations of SPD3, proposals should include a tree planting strategy that extends down to Three Queens Lane, with the undertaking to provide a proportionate number of trees agreed with the council.

Public Art

Considering the quantum of development proposed, a public art proposal should have been developed as part of the full application submission. The public art proposals should be incorporated into a co-ordinating Public Realm Strategy for Redcliff Street and St Thomas Street. It is expected that public art within the study area would be integrated as part of the development of the design of the landscape scheme for the Northern section of St Thomas Street and delivered as part of this development.

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Following substantial discussions with the applicant/ agent of the scheme regarding the design and heritage considerations of the scheme, a revised submission package was formally submitted to the city council in March 2015. Improvements have been made to the scheme whilst there are outstanding design concerns (related to circulation, light, and elevation treatment) that have not been fully addressed in the revised submission and cannot be secured with the applicant/ agent following further officer negotiation. The next stage of the report discusses this in greater detail:

Key Design Issues relating to revised application submission:

1. Site organisation and the quality of external and Internal circulation

A key part of the design negotiation has related to the organisation of the site, and issues related to 'liveability', circulation, sustainability, privacy and massing. The original objective of the design negotiations were as follows:

a) To provide a better approach and circulation for residents by creating a useable central courtyard at ground level. This will provide direct access the internal blocks reducing the number of apartments served solely by the street facing entrances, and reducing the reliance on internal circulation through the ground level internal car park.

b) Revising circulation cores in order to reduce the length of unlit internal corridor circulation, and to increase the legibility and design quality of building elevations within the central semi-private space.

c) Addressing issues of privacy and overlooking by promoting improvements to the scale and massing relating to internal courtyards. Exploring potential to increase the scale of the courtyards together with a reduction in the height of the enclosing buildings, particularly on the northern returns, and promoting the use of dual aspect apartments to maximize a more appropriate arrangement of living rooms onto the street and bedrooms within internal courts where practicable

The revised design proposals have achieved an improved circulation by providing a central space at ground floor level which can be used to directly access the two internal blocks. The recommended positioning of cores has not, however, been taken on board by the applicants design team, and their decision to push the cores into the corners of the block has resulted in very little benefit to the objectives of reducing internal corridor length and increasing natural light levels into internal circulation areas. It is noticeable that on the upper floors where there is a possibility of achieving improved natural light into circulation areas that these opportunities have not been fully taken in the current design proposal.

The location the cores away from a more central position onto the central courtyard has reduced the potential for the these to contribute more significantly to the elevational treatment, to break up the massing of the overall facade treatment, to add visual interest and improve legibility.

The enclosure of the raised internal courtyards has been a key design concern related both to the physical tightness of the space, and policy requirement to minimise the number of single aspect apartments. Opportunities to increase the width of the courtyards or to reduce the number of single aspect apartments are integral to discussions related to revising internal corridors and adjusting site layout, including the setting back or otherwise of the Redcliff Street elevation and positioning of the central space.

Reduction in height of the northern return of the three sided blocks is a further aspect that has been suggested to try and reduce the over enclosure of the courtyard spaces, and It is acknowledged that there has been a reduction of up to two floors from the original design. The block related to St Thomas Street has been reduced from 6 storeys above the first floor courtyard to 4 storeys, whilst the Redcliff Street block has been reduced to 5 storeys above the courtyard. In both cases the return elevations are the same height as the street elevations rather than the taller building heights adjacent

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to the central courtyard. Whilst the complete removal of these returns was promoted the applicants design team has argued that the return close to the Fleece and Firkin is essential for noise protection.

The return within the Redcliff Street block has a close relationship with the existing rear courtyard serving buildings facing Thomas Lane. The previous permission for offices created a similar building height abutting the private space to the north. The application was renewed in 2012, and as such it has been difficult to persuade the applicant to consider a form that would help to provide a more sympathetic response to the existing properties. The proposed design means that the lightwell to the north, which serves both offices and residential windows, will be overshadowed to a greater degree that is currently the case but is no worse than if the existing permission were to be implemented.

The revised application includes an analysis of the Redcliff Street block and the design options which have been considered. For clarity the revised scheme reducing the return elevation from 7 storeys above the courtyard to 5 is the current proposal. An alternative scheme which suggests a further reduction to 3 storeys above the courtyard, and responds better to the lightwell to the north, would provide a preferable scale with regard to the existing adjacent accommodation, but is included for information only. In either case the relocation of the core to the west is welcomed although the perspective drawings indicate that more work is required to refine the elevational treatment.

A final issue relating to the northern returns is the restriction of the roof to areas of visual amenity rather than active roof gardens. This would suggest that there is not a requirement to provide additional parapets or screens to the northern lightwell, which would further increase the height of the buildings. Whilst this is welcomed the plans are not overly convincing as to how this will be managed given direct access to these roof areas from circulation cores.

2. North Facing elevation

The design of the north facing elevation relates to the scale and massing of the new blocks and has a potential adverse visual impact rising up behind the group of smaller historic buildings facing Thomas Lane. Although the massing of this elevation has been apparent, and its likely visibility when viewed from the public space to the south of the Courts has been anticipated, the detail of this elevation was not included within the original submission.

The submitted views analysis includes a view looking along Thomas Lane from Redcliff Street although the image shown is cropped before reaching Decimal House, where the difference in scale is most likely to impact upon the street scene. A view from the public space in front of the Courts has not been included within the views analysis, although it is clear that the new blocks on the east of the site are more likely to rise up and be visible from behind Decimal House and the Seven Stars Public House. As such the detail of what will be a substantial north facing elevation should be considered as an important visible elevation of the scheme.

The elevation submitted as part of the revised package of drawings confirms for the first time the nature of this elevation as an unrelieved rear facade. The eight storey element directly behind Decimal House will have the greatest impact in massing terms, and is primarily designed as a blank facade of banded grey and cream brickwork with the uppermost storey clad in power coloured standing seam metal.

The upper parts of the elevation would benefit from greater modelling, attention to detail and additional windows. The addition of a set-in terrace on the upper floor of the Redcliff Street block provides a useful example of what could be achieved, particularly on the upper storey to reduce the extent of the standing seam roof, but might also be introduced on the two floors below in some form. In addition the protruding 6 storey element to the rear of Decimal House could be articulated as a more distinct component of the elevation in order to reduce the scale of the overall mass, rather than as been suggested as a continuation of the banding treatment. Likewise the lift shaft could be picked out as a feature. Notwithstanding the need for greater relief in the elevation, the final choice of

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materials and detailing should respond to the north facing aspect to ensure an appropriate long term appearance that contributes to the overall visible elevation and setting of St Thomas Church.

3. St Thomas Street elevation

Outstanding design issues not resolved

a) the stepping forward of a retained 14 St Thomas Street as part of a rebuilt elevation. The applicants design team had been encouraged to more appropriately incorporate this historic smaller scale element within a street elevation to complement a mix of new build frontages designed to create an overall street scene reflecting the traditional plot widths. It is strongly felt that the design of 14 St Thomas Street whilst worthy of retention, does not merit the approach to position it in front of the new buildings, but should be set within a consistent building line as feature of a mixed street scene.

b) design improvements to the proposed shopfronts including visual separation from the upper floors, and the creation of stronger framing elements incorporating potential for signage. The visualization of St Thomas Street is particularly unsuccessful with regard to the shopfront design which currently detracts from the street scene as a whole. The continuation of the brick piers to ground overstates the dominance of the upper parts of the building and the applied string courses and downpipes do not demonstrate the level of consideration required for a primary frontage. It should be noted that whilst there currently exists a mix of brick styles in the area, choice of brickwork and quality of detailing should be informed by the traditional pallet within the area, even where a more contemporary design is produced.

c) the need to achieve greater design quality is required for the taller bookend element which is intended to provide a visual focus from Victoria Street, and accommodates the vehicle and pedestrian access into the central courtyard space. The details suggested on the elevations and visualisation lack refinement for such a key part of the design and would benefit from greater consideration. The proposed materials – Bath Stone effect GRC, metal framing feature and GRC slats - lack quality and authenticity for such a prominent part of the building. Likewise the 1:50 part elevation submitted lacks any sense of the finer details suggesting how the building will appear in the street. The implied lack of robust detailing at the ground floor, around the vehicle entrance in particular should be questioned particularly in creating corners that may attract graffiti or anti-social behaviour.

The return corner viewed from the south adjacent to the Chinese Supermarket would benefit from greater attention to detail, particularly if this corner is likely to remain as a step forward in the street. Opening up the haulingway at ground floor level adjacent to the Chinese Supermarket will create a better relationship with the street and reduce the likelihood of this being abused as a blind set back.

(Please note that a revised plan has been prepared and secured through the approved plans list for the above modification of the scheme regarding the return corner of the development as viewed from the south).

4. Redcliff Street elevation

The alignment of the Redcliff Street frontage with the corner building to the north is one which remains unresolved in design terms. The presence of services has been argued as a reason for not stepping the building line forward in order to continue a narrowing of the street intended within the earlier Masterplan, however suggestions to better respond to the return elevation of the existing building have also been discounted. This return elevation remains unresolved in the response currently put forward by the applicants design team. A small setting forward of the entire upper floors would be the most simple method of resolving the street frontage, although other suggestions such as the use of balconies or bays to create some stepping forward have also not been incorporated. The existing stepping forward at ground floor cafe level is particularly intrusive in the street and currently provides a convenient location to store bins which adds little to the quality of the street frontage and needs to

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be better resolved through the current proposals. The revised submission includes porch elements that step forward at ground level and a similar approach at the northernmost corner could be used to lessen the extent and adverse impact of the existing blank return.

In broad terms the Redcliff Street frontage is less successful as a residential elevation, and although understood as ultimately providing a background building within the street, the design could do more to reinforce the evolution of Redcliff into a mixed neighbourhood. Greater expression of the use of the building through design elements such as inward opening doors, bays or emphasis of living room spaces would help to better articulate the facade.

As with the St Thomas Street elevation the incorporation of commercial space and lobby areas could be improved by providing a stronger visual break between the ground and upper floors. This would allow greater scope for designing aspects such as signage, porches, setbacks etc. more successfully to respond to street level activity. The materials suggested within the 1:50 details i.e. predominantly Portland Stone effect GRC is questionable both in terms of quality and tone given the context of the city centre and Redcliff Conservation Area Character Appraisal in particular where dominant materials palette is outlined. Both the primary frontages should incorporate natural stone (bath stone and pennant are the local materials). Again there is an implied lack of detail particularly with regard to ground level columns.

Summary of design issues:

In summary, whilst a number of design concerns have not been fully addressed in the revised submission, improvements have been made to the height, massing, elevational treatment and 'liveability' of the scheme as originally submitted. The scheme is considered to be 'on balance' acceptable, where it is not considered that the proposal will create substantial harm to the Redcliffe Conservation Area or to the setting of adjoining listed buildings. Appropriate conditions have been recommended to secure an appropriate public realm solution for this application, a public art plan and the use of high quality materials within the proposed development.

(D) WILL THE ARCHAEOLOGICAL POTENTIAL OF THE SITE BE PRESERVED?

Policy DM31 of the Site Allocations and Development Management Policies, gives great weight to the conservation of heritage assets. Policy BCS22 is also relevant which seeks to safeguard or enhance heritage assets and the character and setting of areas of acknowledged importance.

Whilst the retention of the façade of no 14 St Thomas Street is welcomed, this is a building that is recognised as a Building of Merit in the Redcliffe Conservation Area Character Appraisal. The street frontage that is presented gives the impression of a two-dimensional addition to the street rather than that of a retained historic building. The way that the buildings to either side and behind loom above the façade reinforces this impression. The demolition of the north and south walls of the building, the north wall especially, is regrettable. The superstructure of the latter appears to be of various dates from the 18th century with 19th century additions. The south wall appears to be more recent with only a short section of stone rubble wall.

Whilst it cannot be proved without archaeological exploration, it is highly possible that both these walls lie on earlier, probably medieval antecedents. Property boundaries, once established, tend to become fossilised in the urban landscape and there are a large number of examples where this has been shown to be the case. The previous scheme endeavoured to retain these boundaries, albeit following rebuilding and this principle should be followed with the current application. This will allow no 14 to be read as a three-dimensional structure which can be appreciated from within the building, rather than a two-dimensional façade.

The previous approved scheme at this site achieved the preservation of the Lawditch, a medieval drainage ditch and administrative boundary, which separated the properties on Redcliff Street from those on St

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Thomas Street. This feature dates at least from the first laying out of Redcliffe in the 12th century. This scheme could well cut across the Lawditch, at least in part. It would be preferable if the line of this feature could be respected. The revised scheme indicates that the line of the Lawditch, running through the centre of the site, is now to be retained as an open route, which is an improvement from the original submission, both from the point of view of creating permeability, and because it allows for the preservation of this feature.

The scheme will abut and mask the party walls of the neighbouring buildings to the north. There has already been a broad photographic survey of these walls, which display dates varying from at least the 18th century but which may have earlier antecedents. The demolition of the walls of no 14 apart from the façade, even if they are subsequently rebuilt on the north and south sides, will mean the loss of historic fabric from the 18th century onwards. There must, therefore, be a programme of recording of all historic fabric that is to be lost or masked by this development.

The site as a whole lies in an area of considerable archaeological potential, with both Redcliff Street and St Thomas Street dating to the 12th century, with tenements along the former having been dated archaeologically to the 1st half of the 12th century. While the proposed development does not propose a basement, the foundations for the buildings are likely to have a significant impact upon below-ground archaeology. There will therefore need to be a programme of archaeological work, to include excavation and the associated analysis and publication of the results.

All the above works (recording of the standing walls, the undertaking of an archaeological excavation, together with subsequent watching brief and the publication of the results) should be secured by means of suitable standard conditions (please see recommended conditions at the end of this report).

(E) DOES THE PROPOSAL ADDRESS SUSTAINABILITY OBJECTIVES?

Policies BCS13 to BCS15 of the Bristol Core Strategy addresses sustainability issues within the scheme. Policy BCS13 sets out a requirement for development in Bristol to take into account the impact of climate change. Development is required, by a variety of means, to both mitigate its own impact on climate change and adapt to the effects of climate change. Policy BCS14 sets out the requirement for proposals for renewable and low carbon sources of energy, including encouraging the use of CHP and district heating systems. New development will also be expected to demonstrate that the heating and cooling systems have been selected according to a heat hierarchy through the submission of an energy strategy. Policy BCS15 relates to sustainable design and construction and encourages a sustainability statement to be submitted for new development and Policy BCS16 considers flood risk and water management in new proposals.

BCS 13 Climate change

The proposed development incorporates a number of design efficiency measures that will reduce consumption and emissions, including the use of materials with U-values exceeding those proposed in Building Regulations, improving leakage performance, and glazing with low thermal transmittance factors (U value) and high solar transmission factors (g value). Measures to reduce risk of overheating have also been incorporated into the design. It is recommended that a planning condition is sought to secure the delivery of the energy efficiency measures outlined within the Energy Strategy Report.

Policy BCS 13 states that Development should mitigate climate change through measures including the use of decentralised, renewable and low-carbon energy supply systems.

In advice provided at the pre-application stage it was noted that Bristol City Council plans to install a district heating main along Redcliff Street in the near future and therefore the development will be required to include the relevant infrastructure to enable the site to connect in to the network at a later

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date. The documents submitted state that “whilst BCC are looking to develop district heating networks, there is currently no district heat network to serve the development and therefore the only appropriate strategy would be based on an individual building solution”.

The current proposal is for electric panel heaters in individual dwellings. It is contended that this is the most suitable and economically viable heating solution for the proposed development. This form of heating system is located at the bottom of the heating hierarchy in terms of energy efficiency, carbon emissions and potentially the cost to the end-user.

The BCC Energy Services team have considered the applicant’s justification for the selection of electric heaters and detailed discussions have taken place with regards to justifying other technologies set out in the Energy Strategy. Following considerable debate, it is considered that the development does not fully address policy requirements where major development in Heat Priority Areas will be expected to incorporate, where feasible, infrastructure for district heating. In the view presented by the Energy Services team with input from external consultants, infrastructure for district heating is considered feasible and agreement could not be reached on the reasons provided by the applicant to demonstrate that this was not the case.

Adaptation

Development should include ways to mitigate extreme temperatures brought about by climate change. This should include planting, canopies and awnings for shade. Development should include blue & green infrastructure to both mitigate the urban heat island and reduce surface water runoff.

The application documents indicate that some planting will occur in courtyards and at roof level. Policy DM29 requires development to incorporate opportunities for green infrastructure such as green roofs, green walls and green decks. Further details are recommended to be provided through condition in order to demonstrate that the use of planting, green roofs / walls has been fully considered and maximised, in order to minimise run-off, the urban heat island effect, and temperatures within the building under future climatic conditions.

BCS 14 Sustainable energy

The renewables contribution, whilst below the 20% requirement of Policy BCS14, is an improvement on the original application submission and the overall reduction beyond part L taking into account both energy efficiency measures and the contribution from 126 solar PV panels is 23.83%. It is recommended to condition the development to be built out in accordance with this target, to be demonstrated through final calculations submitted prior to occupation.

Summary of Sustainability Issues:

In summary, the proposal is considered to be ‘on balance’ acceptable with regards to sustainability issues. Improvements have been made to the renewable contribution beyond the original application submission whilst it is noted that provision for a District Heating connection will not be provided as part of this scheme. Following detailed discussions with the applicant/ agent it is understood that there is a commitment to provide electric heating within the proposal. Appropriate conditions are recommended to secure the solar PV solution at the site and the requirements set out in the submitted energy strategy.

(F) DOES THE PROPOSAL SATISFACTORY ADDRESS TRANSPORT AND MOVEMENT ISSUES?

Policy DM23 provides general transport criteria to be considered as part of all development proposals. It requires developers to ensure that their proposals are sustainable in transport and movement terms, and to give consideration to parking issues, highway safety, congestion, and the benefits of traffic

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calming. Policy BCS10 of the Bristol Core Strategy also encourages proposals to be located where sustainable transport patterns can be achieved. Proposals should minimise the need to travel, especially by private car, and maximise opportunities for the use of walking, cycling and public transport.

Principle

There is no Transport Planning objection in principle to the proposal for residential units and commercial space in this location; this is supported within SPD3 and the Central Area Action Plan.

Obligations

SPD3 sets out obligations expected of the development in this area, and includes the following: strategic pedestrian / cycle improvements (including King Street Bridge), strategic highways – Redcliffe Way and Local Public Realm. Following detailed discussions, it is not considered reasonable to pursue a contribution for the King Street Bridge. The provision of a car club space and car should be secured and a S106 agreement to secure the waiting restrictions required to undertake the Traffic Regulation Order for a car club space and loading bays to the exterior of the site, where £3000 is required.

Trip Generation

Trips associated with the site have been assessed using TRICs traffic modelling. This shows that a comparable number of vehicular trips with the residential to the approved office use and the existing (informal) use as car parking. Following revisions to the design the parking provision has reduced and vehicular trips will be reduced.

There is a low level of proposed car parking and the site is well located for a good range of services and public transport, so car journeys could be minimised. However, good alternative facilities need to be made available to encourage alternative modes of transport and create an improved pedestrian and cycling environment, and discourage the use of the private car.

These would be expected to include (but not be limited to) the following:

- Permeability of the site
- Good quality pedestrian environment
- Accessibility to services and public transport
- Good quality cycle parking to Local Plan standards
- Measures to reduce the speeds of vehicular traffic to 20mph
- Provision of car club car to reduce the need to own and use a private car
- Travel planning and availability of information

Redcliff Street frontage

The opportunity to realign the Redcliff Street building line frontage has not been taken. This results in a potentially unsafe corner at the main pedestrian access point to Residential Entrance A. Officers have raised no objection in principle to the stopping up of this area of highway, given the aspirations to narrow Redcliff Street and create a better environment for pedestrians and cyclists in Redcliff Street as outlined in SPD3.

However, following detailed discussions with the applicant/ agent, negotiations have not been successful to realign the Redcliff Street building line with the residential development to the north of the site. Justification has been provided regarding substantial underground service provision in the street, and it is noted that in this location difficulties would arise from a realignment of the building line. To this end, the footway to the frontage of the site will be reinstated to a suitable standard.

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St Thomas Street

The vehicular access is to be retained in its existing location. The width of the access allows two cars to pass, which is welcomed as this removes queuing vehicles from the footway and junction.

To reduce conflict at the junction and make drivers more aware of the presence of an increase in movements at the junction, a speed table is proposed. This will need to be sensitive to the conservation area and further details will be required through condition. The frontage in St Thomas Street must be reinstated across its entire length using appropriate materials. These works will be secured through a condition.

Permeability

Permeability within the site is important to ensure accessibility on foot and reduce distances needed to travel. The revised submission providing two additional cores within the development (four now provided in total) is an improvement on the original application submission.

Highway Obstructions

Doors on the buildings are shown to open outwards, but this is contrary to the Highway Act. These should all be altered to open inwards, or be recessed; this matter will be dealt with via condition.

Parking and Servicing

Vehicular access to the site will be provided from St Thomas Street into a new semi-private but publically accessible central courtyard area from which each block has a separate, secure parking area. The St Thomas Street block features 20 car parking spaces including 3 disabled bays and 1 motorcycle bay. 3 of these parking spaces allow for electric vehicle charging. In addition, secure cycle parking is provided for 116 cycles.

The Redcliff Street block features parking for 8 vehicles and features 2 disabled parking bays and 78 cycle parking spaces. 3 of these parking spaces allow for electric vehicle charging. New residential entrances to the two internal blocks (secured through revisions to the scheme) can also be accessed from the courtyard area as well as further secure cycle parking for both residents and visitors.

The development will not benefit from residents' parking permits and this will need to be made clear in any future marketing information and travel packs made available to prospective residents by the Travel Plan coordinator.

Cycle parking

In total, 210 cycle spaces are provided with 196 secure cycle parking spaces provided for residents and 16 spaces for visitors and the proposed retail units.

The proposed provision meets the minimum standard and is considered to be acceptable.

Refuse and Servicing

Following revisions to the scheme, the waste storage areas are considered to be accessible and adequately sized for the commercial waste and residential waste. A condition will secure the refuse provision and ensure that no waste is left on the highway at any time.

Servicing is proposed to take place on street, this is considered to be acceptable as this comprises the current arrangements at the site.

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Travel Planning

A car club space and car will be required and the applicants will be expected to secure this directly with car club providers and provide membership for three years to each dwelling this will be secured through condition and legal agreement. The travel plan contains no targets or monitoring and a condition is recommended to request greater details through the provision of a new travel plan.

(G) DOES THE PROPOSAL SATISFACTORILY ADDRESS FLOOD RISK ISSUES?

Flood issues are considered in the Technical Guide to the National Planning Policy Framework, March 2012. The site is located within flood zone 2, where it is considered to be at a medium risk of flooding. A full Flood Risk Assessment (FRA) has been submitted in addition to a sequential test. The Environment Agency (EA) has raised no objection to the proposed development, provided that a number of conditions are included within the consent. Flood warning and evacuation measures are also recommended to be secured by condition.

Sequential Test:

The Local Planning Authority has produced a Flood Risk Sequential Test Practice Note that amongst other issues sets out the Authority's requirements with regard to defining an appropriate sequential test search area for developments. The practice note recognises that Bristol, particularly Bristol City Centre, has a number of existing buildings that have reached the end of their useful life for their present use and would benefit from conversion to other uses, but are at risk of flooding either at present or with climate change. It is also recognised that it would be harmful to the continued regeneration and sustainable development of Bristol for these buildings to remain vacant. These are identified within the Central Area Action Plan and as a site for development within the adopted SPD3. Therefore, due to the importance of the regeneration of this area, the scope of the sequential test has been defined as the regeneration area boundary within SPD3.

The sequential test provides a full list of development sites in Redcliffe and its vicinity, and identifies the suitability, flood risk and constraints relevant to delivering the quantum of development sought on the site. The document demonstrates the absence of any suitable sites within the area that are sequentially preferable.

Flood Risk:

To reduce the impact of flooding to the development, a number of mitigation measures have been proposed including flood resilience measures. With the inclusion of key conditions recommended by the Environment Agency, it is considered that the proposal is acceptable with regards to flood risk.

(H) IS THE PROPOSAL ACCEPTABLE WITH REGARDS TO GROUND CONTAMINATION ISSUES?

Policy BCS23 of the Core Strategy and the National Planning Policy Framework set out the requirements for considering development on contaminated land.

Further investigation is requested following demolition as contamination of soils and potentially groundwater were encountered on a previous investigation in 2002. Ground gas was also encountered. Soil investigations from 2002 are not considered acceptable for submission due to the changes in guidance, guideline values and testing methods adopted since this time. The ground gas situation may have also changed significantly in the past 12 years and very little is known about the potential impacts to ground and controlled waters.

Pollution Control officers agree with the findings of the contaminated land report that recommend a site specific further investigation. Conditions are recommended for inclusion.

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(I) HAS AN AIR QUALITY MANAGEMENT PLAN BEEN PRODUCED FOR THE SITE?

Policy BCS10 Transport and access improvements and BCS23 pollution of the Bristol Core Strategy consider air quality in new development.

The air quality assessment has been reviewed and there are no concerns with regards to air quality. An assessment has been carried out into the potential impact from demolition and construction dusts and it is recommended that a construction management plan is secured via condition.

(J) WOULD THE PROPOSAL HAVE AN IMPACT UPON ANY NATURAL SPECIES OR HABITATS ON THE SITE?

Policy DM19 states that account will be taken of the retention and protection of existing natural features and habitats. Policy BCS9 states that the integrity and connectivity of strategic green infrastructure will be maintained, protected and enhanced.

The proposals in the Design and Access statement to provide community amenity space are noted. There are also opportunities to provide living (green/brown) roofs to provide habitat for wildlife. Living roofs also contribute towards Sustainable Urban Drainage Systems (SuDS). Landscaping of the site should predominantly employ native species of local provenance including berry and fruit-bearing tree, hedgerow and shrub species for birds and nectar-rich flowering plants for invertebrates. Recommended conditions have been included at the end of the report.

(K) IS THE PROPOSED DEVELOPMENT VIABLE AND DOES IT PROVIDE AN APPROPRIATE AFFORDABLE HOUSING CONTRIBUTION?

Government policy on planning obligations is set out in Paragraphs 203 to 205 of the National Planning Policy Framework (NPPF) (March 2012). In addition, Regulation 122 of the Community Infrastructure Levy Regulations 2010 (as amended) sets out the following legal tests that must be satisfied in order for obligations to be required in respect of development proposals:

- 'The obligation must be necessary to make the proposed development acceptable in planning terms;
- The obligation must be directly related to the proposed development;
- The obligation must be fairly and reasonably related in scale and kind to the proposed development.'

Furthermore, the government's Planning Practice Guidance was a month ago and states:

'Where affordable housing contributions are being sought, obligations should not prevent development from going forward. (Para 004 Reference ID: 23b-004-20150326).'

The proposed development would be required to provide the following obligations:

- The provision of affordable housing in accordance with Core Strategy Policy BCS17. This requires the provision of up to 40% affordable housing subject to scheme viability.
- A contribution of £3,000 to enable loading site access an on-street car club bay

Whilst there are no other planning obligations, the development will be required to make a significant Community Infrastructure Levy (CIL) payment, as set out elsewhere in this report. In addition, highway and public realm improvements and public art works will be secured via planning conditions, and will be delivered by the developer.

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Affordable Housing

In simple terms, a development is considered to be viable if the Residual Land Value (RLV) of the development is greater than the site value. The RLV is calculated by ascertaining the value of the completed development, and subtracting from this all the costs involved in bringing the development forward (e.g. build costs, professional fees, legal costs, financing costs etc.) and the developers profit.

The applicant has claimed that, to remain viable, the proposed development is unable to provide any affordable housing, and has submitted a viability appraisal and detailed commentary to support their claim.

Officers have commissioned DVS (the property arm of the governments Valuation Office Agency) to assess the viability information and advise the Council as to whether the applicants claim is reasonable. It should be noted that DVS have a detailed understanding of development costs and values in the City Centre, having advised the Council on a number of City Centre schemes recently, including Plot ND7 at Temple Quay North, where their involvement resulted in the level of affordable housing increasing from the 0% originally offered by the applicant to 15%.

Having assessed the values and costs associated with the proposed development, and undertaken their own appraisal, DVS have advised that the proposed development cannot provide 40% affordable housing as to do so would result in a significant negative RLV. However, DVS do not agreed that the proposed scheme is unable to make any contribution towards affordable housing. They advise that recent residential sales values achieved in the city centre indicate that slightly higher residential values could be achieved than those identified by the applicant. These increased values result in the proposed development showing a positive RLV of £240,000, meaning that an affordable housing contribution could be provided.

On the basis of this advice, officers consider that an affordable housing contribution of £240,000 is required and that this should be secured by way of a Section 106 Agreement. Following discussions with the applicant, this contribution has been agreed.

Traffic Regulation Orders (TRO's)

The proposed development would require changes to waiting restrictions in surrounding roads (particularly Redcliff Street and St. Thomas Street). Changes to waiting restrictions require an amendment to TRO's, which can only delivered by the Council in its role as highway authority. In order to process amendments to TRO's the Council charges a set fee of £3,000 and this is set out in the Planning Obligations Supplementary Planning Document (Adopted on 27 September 2012).

The applicant has agreed to make this payment.

Conclusion on viability issues:

Based on the advice received from DVS in respect of affordable housing, and the set costs identified in the Planning Obligations Supplementary Planning Document for amending TRO's, officers consider that the following planning obligations acceptable mitigate the impact of the proposed development:

- A contribution of £240,000 towards affordable housing; and
- A contribution of £3,000 towards amendments to TRO's

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In summary, the regeneration of a large, long term vacant site in the Redcliffe area for a mixed use residential and retail development is welcomed. It is noted that there are a number of outstanding design issues which have not been fully resolved in the revised submission and as such the City Design Group maintain an objection to the scheme as it currently stands with regard to the visual impact of the taller block behind the Seven Stars public house and details related to the elevational treatment onto Redcliff Street and St Thomas Street.

The provision of the necessary infrastructure to secure District Heating at the site also remains unresolved, as the applicant has demonstrated a commitment to an electric heating solution in the proposal. On balance, taking into account the concerns that remain with the application against the overall regeneration benefits of the proposed development, which includes a contribution of £240,000 towards affordable housing, it is recommended that the application is approved subject to the signing of a legal agreement and detailed planning conditions.

COMMUNITY INFRASTRUCTURE LEVY

How much Community Infrastructure Levy (CIL) will the development be required to pay?

The CIL liability for this development is £802,272.

RECOMMENDED GRANT subject to Planning Agreement**Time limit for commencement of development**

1. Full Planning Permission

The development hereby permitted shall begin before the expiration of three years from the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

Pre commencement condition(s)

2. Land affected by contamination - submission of remediation scheme

No development shall take place until a detailed remediation scheme to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property and the natural and historical environment has been prepared, submitted to and approved in writing by the Local Planning Authority. The scheme must include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

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3. Further site investigation required: Site Characterisation

Following demolition of the existing buildings a further site investigation will be required to establish site conditions at the site. The results of this investigation should be considered along with the Desk Study and Preliminary Risk Assessment prepared by Arup, dated July 2014. Based on the findings of the two reports a site specific risk assessment should be carried out to assess the nature and extent of the contamination on the site, whether or not it originates on the site. The contents of the scheme should be submitted to and be approved in writing by the Local Planning Authority. The investigation and risk assessment must be undertaken by competent persons and a written report of the findings must be produced.

This must be conducted in accordance with DEFRA and the Environment Agency's 'Model Procedures for the Management of Land Contamination, CLR 11'.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors

4. Land affected by contamination - implementation of approved remediation scheme

In the event that contamination is found, no development other than that required to be carried out as part of an approved scheme of remediation shall take place until the approved remediation scheme has been carried out in accordance with its terms. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

Following completion of measures identified in the approved remediation scheme, a verification report that demonstrates the effectiveness of the remediation carried out must be produced, and be approved in writing of the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

5. To ensure completion of a programme of archaeological works

No building shall be occupied until the building survey, site investigation and post investigation assessment has been completed in accordance with the programme set out in the Written Scheme of Investigation approved under condition (000) and the provision made for analysis, publication and dissemination of results and archive deposition has been secured.

Reason: To ensure that archaeological remains and features are recorded and published prior to their destruction

6. To secure the recording of the fabric of buildings of historic or architectural importance

No redevelopment or refurbishment of 25 Redcliff Street or 14 St Thomas Street shall take place until the applicant/developer has recorded those parts of the building which are likely to be disturbed or concealed in the course of redevelopment or refurbishment. The recording to be carried out by an archaeologist or archaeological organisation approved by the Local Planning Authority.

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Reason: To ensure that features of archaeological or architectural importance within a building are recorded before their destruction or concealment.

7. To ensure a suitable foundation design to preserve archaeological features has been secured

No work on site (including site clearance) shall take place until a detailed design for the foundations, piling configuration, drainage and services) together with a method statement for their construction has been submitted to and approved in writing by the Local Planning Authority. The development hereby approved shall only take place in accordance with the approved detailed scheme.

Reason: To ensure the preservation in situ of archaeological features of identified importance.

8. The provision of a methodology for demolition works

No development, including demolition, shall take place until the applicant/developer has submitted a method statement for the demolition of all buildings within the area indicated on plan number (0000). No demolition shall take place until the method statement has been approved.

Reason: to ensure that underlying archaeological material is not disturbed during the demolition programme and that the retained façade of no 14 St Thomas Street is not compromised.

9. To ensure implementation of a programme of archaeological works

No development shall take place within the area indicated on plan number (0000) until the applicant/developer has secured the implementation of a programme of archaeological work, to include detailed recording of the standing building, archaeological excavation, archaeological watching brief and the publication of the results, in accordance with a Written Scheme of Investigation which has been submitted by the developer and approved in writing by the Local Planning Authority.

The scheme of investigation shall include an assessment of significance and research questions; and:

1. The programme and methodology of site investigation and recording
2. The programme for post investigation assessment
3. Provision to be made for analysis of the site investigation and recording
4. Provision to be made for publication and dissemination of the analysis and records of the site investigation
5. Provision to be made for archive deposition of the analysis and records of the site investigation
6. Nomination of a competent person or persons/organisation to undertake the works set out within the Written Scheme of Investigation.

Reason: To ensure that archaeological remains and features are recorded prior to their destruction.

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10. Approval of road works necessary

No development shall take place until details of the following works to the highway have been submitted to and approved in writing by the Local Planning Authority:

1. Refurbishment of footways and carriageway in St Thomas Street to the extents shown on the attached plan using appropriate conservation materials
2. Raised table at the junction of St Thomas Street and St Thomas Lane
3. Refurbishment of footway in Redcliff Street to the frontage of the development
4. Car club space Redcliff Street
5. Associated ancillary works including (but not limited to) lighting, lining, signing, street furniture, drainage and resurfacing.

The building hereby permitted shall not be occupied until these works have been completed in accordance with the approved details.

Reason: To ensure that all road works associated with the proposed development are to a standard approved by the Local Planning Authority and are completed before occupation.

11. Sustainable drainage system (SuDS)

The development hereby approved shall not commence until a Sustainable Drainage Strategy and associated detailed design, management and maintenance plan of surface water drainage for the site using SuDS methods has been submitted to and approved in writing by the Local Planning Authority. The approved drainage system shall be implemented in accordance with the approved Sustainable Drainage Strategy prior to the use of the building commencing and maintained thereafter for the lifetime of the development.

Reason: To prevent the increased risk of flooding by ensuring the provision of a satisfactory means of surface water disposal and that the principles of sustainable drainage are incorporated into this proposal and maintained for the lifetime of the proposal.

12. Flood evacuation plan - commercial property

No development shall take place until the applicant has submitted to and had approved in writing by the Local Planning Authority a Flood Warning and Evacuation Plan (FEP). This Plan shall include the following information:

- * command & control (decision making process and communications to ensure activation of FEP);
- * training and exercising of personnel on site (H& S records of to whom and when);
- * flood warning procedures (in terms of receipt and transmission of information and to whom);
- * site evacuation procedures and routes; and
- * provision for identified safe refuges (who goes there and resources to sustain them).

The FEP shall be reviewed at intervals not exceeding 3 years, and will form part of the Health & Safety at Work Register maintained by the applicant.

Reason: To limit the risk of flooding by ensuring the provision of a satisfactory means of flood management on the site.

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13. Flood evacuation plan - residential property

No development shall take place until the applicant has submitted to and had approved in writing by the Local Planning Authority a Flood Warning and Evacuation Plan (FEP). This Plan shall include the following information:

During Demolition/Construction Process

- * command & control (decision making process and communications to ensure activation of FEP);
- * training and exercising of personnel on site (H& S records of to whom and when);
- * flood warning procedures (in terms of receipt and transmission of information and to whom);
- * site evacuation procedures and routes; and,
- * provision for identified safe refuges (who goes there and resources to sustain them).

During Occupation of Development

- * occupant awareness of the likely frequency and duration of flood events;
- * safe access to and from the development;
- * subscription details to Environment Agency flood warning system, 'Flood Warning Direct'.

Reason: To limit the risk of flooding by ensuring the provision of a satisfactory means of flood management on the site

14. Provision of swift boxes/ bricks

Prior to the commencement of development details shall be submitted providing the specification, orientation, height and location for built-in bird nesting opportunities. This shall include twelve built-in swift boxes.

The details thereby approved shall be carried out in accordance with that approval.

Reason: To help conserve legally protected birds which include priority species.

15. Green Roof - Landscaping Scheme

The development hereby permitted shall not take place until there has been submitted to and approved in writing by the Local Planning Authority a landscaping scheme for the green roof areas of the buildings, hereby approved, including proposed planting details and a management scheme for future maintenance of the roofs. The approved scheme shall be installed prior to occupation of the buildings and maintained in perpetuity unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that the external appearance of the building is satisfactory and that sustainable principles are incorporated into this proposal.

16. Construction Management Plan

No development shall take place until a site specific Construction Environmental Management Plan has been submitted to and approved in writing by the Council. The plan must demonstrate the adoption and use of the best practicable means to reduce the effects of noise, vibration, dust and site lighting.

The Construction Environmental Management Plan should also include but is not limited to reference to the following:

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- Details of measures to reduce vehicle speeds in access road from St Thomas Street.
- Procedures for maintaining good public relations including complaint management, public consultation and liaison
- Arrangements for liaison with the Council's Pollution Control Team
- All works and ancillary operations which are audible at the site boundary, or at such other place as may be agreed with the Local Planning Authority, shall be carried out only between the following hours:
 - 08 00 Hours and 18 00 Hours on Mondays to Fridays and
 - 08 00 and 13 00 Hours on Saturdays and;
 - at no time on Sundays and Bank Holidays.
- Deliveries to, and removal of plant, equipment, machinery and waste from the site must only take place within the permitted hours detailed above.
- Mitigation measures as defined in BS 5528: Parts 1 and 2: 2009 Noise and Vibration Control on Construction and Open Sites shall be used to minimise noise disturbance from construction works.
- Procedures for emergency deviation of the agreed working hours.
- Bristol City Council encourages all contractors to be 'Considerate Contractors' when working in the city by being aware of the needs of neighbours and the environment.
- Control measures for dust and other air-borne pollutants. This must also take into account the need to protect any local resident who may have a particular susceptibility to air-borne pollutants.
- Measures for controlling the use of site lighting whether required for safe working or for security purposes.

Reason: In the interests of highway safety and residential amenity.

17. Sound Insulation

No development shall take place until a scheme of noise insulation measures for all residential accommodation has been submitted to and approved in writing by the Council.

The noise insulation measures shall be designed to achieve noise insulation to a standard that nuisance will not be caused to the occupiers of residential accommodation by noise from the following:-

Noise from the Fleece

Noise from the commercial units on the ground floor of the development

Traffic and street noise

The scheme of noise insulation measures shall take into account the recommendations detailed in the Noise Assessments submitted with the application and the provisions of BS 8233: 2014 "Guidance on sound insulation and noise reduction for buildings".

The approved details shall be implemented in full prior to the commencement of the use permitted and be permanently maintained. The works will need to be regularly checked by a competent acoustic consultant throughout the construction of the development and inspected by an authorised officer of Bristol City Council's Pollution Control Team prior to the commencement of the use of the development.

Reason: In the interests of protecting future residential amenity.

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18. Further details of photovoltaic system

Prior to the commencement of development, the following information shall be submitted to and approved in writing by the Local Planning Authority.

- A final roof plan showing the number and area of panels
- the electrical rating of the PV array in kilowatts peak (kWp)
- the postcode region
- the array pitch
- the array orientation
- kWh/kWp (Kk) from the appropriate location specific table
- the shading factor of the array (SF) according to any objects blocking the horizon

Reason: To ensure that the development incorporates renewable energy and contributes to meeting targets to reduce carbon dioxide emissions as set out in the approved Energy Strategy dated 9th March 2015 (Applied Energy).

19. Large scale details of the proposed buildings fronting Thomas Street and Redcliff street

Detailed drawings including plans, sections and elevations at a relevant scale between 1:5 and 1:20 of the following shall be submitted to and be approved in writing by the Local Planning Authority before the relevant part of work is begun. The works shall be carried out in accordance with that approval.

- a) Typical detailing of all facing materials (brick, stone, timber) including coursing, bonding, pointing and other decorative features.
- b) Typical details of all types of doors and window opening including sills, lintels, surrounds, reveals and design of external doors and windows.
- c) Typical details of all balcony types and treatments
- d) Typical details of bay projections, porches and canopies
- e) Typical details of eaves, soffits, overhangs, ridge, parapets, coping
- f) Typical details of rain water goods
- g) Typical details of junctions between materials

Reason: In order to ensure the quality of the design and external appearance of the building is of high quality and compliant with the local Plan.

20. Large scale details of the proposed north elevation of buildings

Detailed drawings including plans, sections and elevations at a relevant scale between 1:5 and 1:20 with particular focus on parts of the north elevation which are visible from the surrounding public realm, shall be submitted to and be approved in writing by the Local Planning Authority before the relevant part of work is begun. The works shall be carried out in accordance with that approval.

- e) Three dimensional qualities of the visible upper parts of the building
- a) Typical brick and stone detailing including coursing, bonding, pointing and other decorative features.
- b) Typical details of all types of any doors and window opening including sills, lintels, surrounds, reveals and design of external doors and windows.
- c) Typical details of upper storey cladding, together with any safety rails, access ladders, parapets and copings.
- d) Typical details of rain water goods
- e) Typical details of junctions between materials

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Reason: In order to ensure the visual impact of the design and external appearance of the building is of high quality and compliant with the local Plan.

21. To ensure a suitable design to preserve the facade of No. 14 St Thomas Street has been secured

No construction work on site shall take place until, a detailed design for the preservation in situ of the building facade with a method statement for construction, shoring and the detailed integration of building fabric has been submitted to and approved in writing by the Local Planning Authority. The development hereby approved shall only take place in accordance with the approved detailed scheme.

Reason: To ensure the preservation in situ of historic facade features of identified importance.

22. Provision of material samples/sample panels on site for inspection

A reference panel of all external materials including wall facing materials, external cladding, plinth, external doors and windows including frames, sills, lintels and surrounds, door/window, decorative features, eaves, rainwater goods demonstrating the colour, texture, face bond and pointing shall be erected at an appropriate location on site and approved in writing by the Local Planning Authority before the relevant parts of the works are commenced.

The development shall be completed in accordance with the approved reference panel before the building is occupied. The approved reference panel should be retained until the completion of the building.

Reason: In order to ensure the quality of the design and external appearance of the building is of high quality and compliant with the Development Plan.

23. Shopfront Design Guide

Prior to the commencement of the relevant parts of the development, the developer in liaison with Bristol City Council's Urban Design Officer to prepare a Shopfront Design Guide for the site to ensure that the future installation of shopfronts into the St Thomas street and Redcliffe Street facades achieve an appropriate design quality in the context of the overall elevational treatment of the buildings. The Shopfront Design Guide shall include:

- a) The extent of the shopfront, fascia and surrounds within the overall building facade,
- b) The design proportions used within new shopfronts
- c) The integration of signage
- d) Design examples and suggested materials
- e) Typical details associated with security components
- f) Typical details of signage illumination and lighting

The design guidance shall thereafter be submitted to and approved in writing by the Local Planning authority.

Reason: In order to ensure the quality of the design and external appearance of the building is of high quality and compliant with the Development Plan.

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24. Landscape

Detailed design: Detailed design for hard and soft landscape works for the associated public realm works within St Thomas Street and Redcliffe Street (identified on plan no. 14/006/ P-003) and the central courtyard should be submitted and agreed with the Local Planning Authority prior to the commencement of relevant works. These works should be delivered in accordance with an agreed plan and phasing which is linked to the occupation of the building of the relevant phase.

Landscape Details: Detailed drawings including plans, sections and elevations at a relevant scale between 1:5 and 1:20 of the following shall be submitted to and be approved in writing by the Local Planning Authority before the relevant part of work is begun. The works shall be carried out in accordance with that approval.

- a) Construction details relating to paving materials (including the relaying of traditional setts, cast iron kerbing, pennant paving and hauling way blocks)
- b) Details of street furniture including seating, lamps.
- c) Details of associated public art
- d) Full details of tree pit construction in highway and soft landscape areas showing relationship to the surrounding highway and footway paving, tree grills where used, nature of growing medium, size of pits, tree support, method of anchoring and means of irrigation to ensure the provision of optimum growing conditions for newly planted trees.

Reason: To protect and enhance the character of the site and the area and to ensure it's appearance is satisfactory.

25. Landscape Sample Panel

Sample panels of all external surfacing materials including paving, kerbs, edging, showing jointing details shall be erected at an appropriate location on site and approved in writing by the Local Planning Authority before the relevant parts of the work are commenced. The development shall be completed in accordance with the approved sample before the relevant phase is occupied in accordance with the phasing plan submitted. The approved sample should be retained until the completion of works.

Reason: To protect and enhance the character of the site and the area and to ensure it's appearance is satisfactory.

26. Maintenance Plan

A detailed maintenance plan to ensure establishment of the soft landscape works for the scheme over the first 5 years should be provided and agreed with the local planning authority prior to the commencement of soft landscape works. Once agreed, the soft landscape should be maintained in accordance with the agreed plan.

Reason: To protect and enhance the character of the site and the area and to ensure it's appearance is satisfactory.

27. Public Art: Public Art Plan

Prior to the commencement of the relevant parts of the development, the developer shall appoint a suitably qualified public art consultant/curator (in liaison with Bristol City Council's Public Art Officer) to prepare a Public Art Plan for the site with the focus on:

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- The central courtyard within the development that connects the development with St Thomas Street.
- The landscape / public highway improvement works to St Thomas Street, St Thomas Street East, and Redcliff Street
- The detailed design of all building elevations onto Redcliffe Street, St Thomas Street and Thomas Lane

The Public Art Plan shall set out the process to be used to commission and integrate public art into the agreed locations above, along with:

- a programme for the development of the public art works (schedule),
- details of the commission project management
- artist's procurement process
- details of art work maintenance and care

The proposal shall thereafter be submitted to and approved in writing by the Local Planning authority along with the detailed landscape and building elevation plans prior to the commencement of the relevant works unless otherwise agreed in writing with the Local Planning Authority.

The scope, programme and design of the public art proposed shall be agreed in writing with Local Planning Authority.

Reason: To ensure that public art is integrated into the design of the development.

Pre occupation condition(s)**28. Travel Plans - not submitted**

No building or use hereby permitted shall be occupied or the use commenced until a Travel Plan comprising immediate, continuing and long-term measures to promote and encourage alternatives to single-occupancy car use has been prepared, submitted to and been approved in writing by the Local Planning Authority. The approved Travel Plan shall then be implemented, monitored and reviewed in accordance with the agreed travel Plan Targets to the satisfaction of the council.

Reason: In order to deliver sustainable transport objectives including a reduction in single occupancy car journeys and the increased use of public transport, walking & cycling.

29. Reinstatement of redundant accessways - shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the existing accesses to the development site has been permanently stopped up and the footway reinstated in accordance with the approved plans.

Reason: In the interests of pedestrian safety.

30. Noise from development

No commencement of use of the individual commercial units at the development shall take place until an assessment on the potential for noise from the commercial unit affecting other residential or commercial properties as part of the development has been submitted to and approved in writing by the Council.

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If the assessment indicates that noise from the development is likely to affect neighbouring residential or commercial properties then a detailed scheme of noise mitigation measures shall be submitted to and approved in writing by the Council prior to the commencement of the development.

The noise mitigation measures shall be designed so that nuisance will not be caused to the occupiers of neighbouring noise sensitive premises by noise from the development.

The noise assessment shall be carried out by a suitably qualified acoustic consultant/engineer and shall take into account the provisions of BS4142: 1997. "Method of rating industrial noise affecting mixed residential and industrial areas" and BS 8233: 1999 "Sound Insulation and Noise Insulation for Buildings - Code of Practice".

The approved scheme shall be implemented prior to the commencement of the use and be permanently maintained thereafter.

Reason: In the interests of protecting residential amenity.

31. Implementation/installation of refuse storage and recycling facilities - shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the refuse store, and area/facilities allocated for storing of recyclable materials, as shown on the approved plans have been completed in accordance with the approved plans. Thereafter, all refuse and recyclable materials associated with the development shall either be stored within this dedicated store/area, as shown on the approved plans, or internally within the building(s) that form part of the application site. No refuse or recycling material shall be stored or placed for collection on the public highway or pavement at any time.

Reason: To safeguard the amenity of the occupiers of adjoining premises, protect the general environment, and prevent obstruction to pedestrian movement, and to ensure that there are adequate facilities for the storage and recycling of recoverable materials.

32. Completion of vehicular access - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the means of vehicular access has been constructed and completed in accordance with the approved plans and the said means of vehicular access shall thereafter be retained for access purposes only.

Reason: In the interests of highway safety.

33. Completion of Pedestrians/Cyclists Access - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the means of access for pedestrians and/or cyclists have been constructed in accordance with the approved plans and shall thereafter be retained for access purposes only.

Reason: In the interests of highway safety.

34. Installation of vehicle crossover - shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the vehicular crossover(s) has been installed and the footway has been reinstated in accordance with the approved plans.

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Reason: In the interests of pedestrian safety and accessibility.

35. Prior to the occupation of development, a car club operator shall be appointed to provide free membership to all of the residents of the development from first occupation to a period of three years thereafter.

Reason: to provide car club access and alternative transport facilities for the residents of the development thereby reducing the need to own a car."

36. The development hereby permitted shall not be commenced until such time as details of a flood barrier/gate on the entrance to the car park area has been submitted to, and approved in writing by, the local planning authority. The flood barrier/gate should provide effective flood protection up to a level of at least 9.4m AOD.

Reason: To reduce the risk of flooding to the proposed development and future occupants.

37. The development permitted by this planning permission shall only be carried out in accordance with the approved Flood Risk Assessment (FRA) by ARUP dated 16 September 2014 and the following mitigation measures detailed within the FRA:

1. Flood-resilience measures to be incorporated into the ground floor of the development as outlined in Section 3.3 on page 9 of the FRA.
2. Finished floor levels to be set as per drawing no. 14/006 P-100.

Reason

1. To reduce the impact of flooding on the proposed development and future occupants.
2. To reduce the risk of flooding to the proposed development and future occupants.

38. Land affected by contamination - Reporting of Unexpected Contamination

In the event that contamination is found at any time when carrying out the approved development that was not previously identified it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with the requirements of Condition and where remediation is necessary a remediation scheme must be prepared in accordance with the requirements of Condition ****, which is to be submitted to and be approved in writing by the Local Planning Authority.

Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority in accordance with condition ****.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

39. Completion and maintenance of cycle provision - shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the cycle parking provision shown on the approved plans has been completed, and thereafter, be kept free of obstruction and available for the parking of cycles only.

Reason: To ensure the provision and availability of adequate cycle parking.

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40. Deliveries (A1 and A3 use only)

Activities relating to deliveries shall only take place between 08.00 and 20.00 Monday to Saturday and not at all on Sundays or Bank Holidays.

Reason: In the interests of protecting residential amenity.

41. Use of Refuse and Recycling facilities (A1 and A3 use only)

Activities relating to the collection of refuse and recyclables and the tipping of empty bottles into external receptacles shall only take place between 08.00 and 20.00 Monday to Saturday and not at all on Sundays or Bank Holidays.

Reason: In the interests of protecting residential amenity.

42. Noise from plant & equipment

The rating level of any noise generated by plant & equipment as part of the development shall be at least 5 dB below the pre-existing background level as determined by BS4142: 2014 Methods for rating and assessing industrial and commercial sound.

Reason: In the interests of protecting residential amenity.

43. Odour Management Plan (A3 use only)

No commencement of use of the individual commercial units at the development shall take place until there has been submitted to and approved in writing, by the Council, an Odour Management Plan, setting out cleaning, maintenance and filter replacement policies. The plan should include a written recording system to record and demonstrate when all such work is carried out.

Reason: In the interests of protecting residential amenity.

44. Details of Extraction/Ventilation System (A3 use only).

No commencement of use of the individual commercial units at the development shall take place until details of ventilation system for the extraction and dispersal of cooking odours including details of the flue, method of odour control, noise levels and noise attenuation measures has been submitted to and approved in writing by the Council.

The approved scheme shall be implemented prior to the commencement of the use and be permanently maintained thereafter.

Reason: In the interests of protecting residential amenity.

45. Completion and maintenance of car/vehicle parking - shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the car/vehicle parking area shown on the approved plans has been completed, and thereafter, the area shall be kept free of obstruction and available for the parking of vehicles associated with the development

Reason: To ensure that there are adequate parking facilities to serve the development.

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46. Sustainable Energy Generation

The specifications and commitments in the approved Energy Strategy dated 9th March 2015 (Applied Energy), including the incorporation of a smart meter with an energy monitor within the development and the installation of a 126 photovoltaic system to achieve the specified target for the reduction in CO2 emissions in the above Energy Strategy, shall be installed prior to the first occupation of the development hereby approved, and shall be implemented in accordance with the approved strategy and retained as operational thereafter.

Reason: To ensure that the development incorporates renewable energy and contributes to meeting targets to reduce carbon dioxide emissions as set out in the approved Energy Strategy dated 9th March 2015 (Applied Energy).

47. Hard and Soft Landscape Works Implementation

The landscaping proposals hereby approved shall be carried out no later than during the first planting season following the date when the development hereby permitted is ready for occupation or in accordance with a programme agreed in writing with the Local Planning Authority. All planted materials shall be maintained for five years and any trees or plants removed, dying, being severely damaged or becoming seriously diseased within 5 years of planting shall be replaced with others of similar size and species to those originally required to be planted.

Reason: To protect and enhance the character of the site and the area, and to ensure its appearance is satisfactory.

48. Public Art Designs

Following the approval of the public art plan, details of the public art commission for the areas identified along with details of their construction, care and maintenance, shall be submitted to the Local Planning Authority for approval prior to the relevant works commencing. The public art work(s) shall be completed in accordance with the plans and programme agreed by the Local Planning Authority and thereafter retained as part of the development, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that public art is integrated into the design of the development.

Post occupation management

49. To secure the conduct of a watching brief during development groundworks

The applicant/developer shall ensure that all groundworks, including geotechnical works, are monitored and recorded by an archaeologist or an archaeological organisation to be approved by the council and in accordance with the Written Scheme of Investigation approved under condition (000)

Reason: To record remains of archaeological interest before destruction.

50. Protection of parking and servicing provision

The areas allocated for vehicle parking, loading and unloading, circulation and manoeuvring on the approved plans shall only be used for the said purpose and not for any other purposes.

Reason: To ensure the provision and availability of satisfactory off-street parking and servicing/loading/unloading facilities for the development.

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51. Restriction of parking level on site

Parking within the development site is to be restricted to the areas allocated on the approved plans and shall not encroach onto areas allocated on the plans for other uses.

Reason: To control the level of parking on the site and to safeguard the uses of other areas.

List of approved plans

52. List of approved plans and drawings

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision.

14/006/P 100 B Proposed Ground Floor Plan, received 9 March 2015
 14/006/ P304 Existing and Proposed North Elevation, received 9 March 2015
 14/006/ P305 Existing and Proposed Internal North Elevation, received 9 March 2015
 14/006/ SK-324 Proposed North Elevation, received 9 March 2015
 14/006/ SK-321 Redcliff St Courtyard Analysis, received 9 March 2015
 14/006/ SK-322 Proposed St Thomas Street Elevation, received 9 March 2015
 14/006/SK-331 St Thomas St Elevation, received 30 March 2015
 14/006/P-001 Site location plan, received 4 November 2014
 14/006/P-002 B Existing site plan, received 9 March 2015
 14/006/P-003 B Proposed site plan, received 9 March 2015
 14/006/P-101 B Proposed first floor plan, received 9 March 2015
 14/006/P-102 B Proposed second floor plan, received 9 March 2015
 14/006/P-103 B Proposed third floor plan, received 9 March 2015
 14/006/P-104 B Proposed fourth floor plan, received 9 March 2015
 14/006/P-105 B Proposed fifth floor plan, received 9 March 2015
 14/006/P-106 B Proposed sixth floor plan, received 9 March 2015
 14/006/P-107 B Proposed seventh floor plan, received 9 March 2015
 14/006/P-108 B Proposed roof plan, received 9 March 2015
 14/006/P-200 B Existing and proposed site sections, received 9 March 2015
 14/006/P-201 B Proposed long sections sheet 1, received 9 March 2015
 14/006/P-202 B Proposed long sections sheet 2, received 9 March 2015
 14/006/P-203 B Proposed cross sections, received 9 March 2015
 14/006/P-300 B Existing and proposed Redcliff Street elevations, received 9 March 2015
 14/006/P-301 B Existing and proposed St Thomas Street elevations, received 9 March 2015
 14/006/P-302 B Proposed block A Internal elevations, received 9 March 2015
 14/006/P-303 B Proposed block B internal elevations, received 9 March 2015
 14/006/P-350 B Material elevation - Redcliff Street, received 9 March 2015
 14/006/P-351 B Material elevation - St Thomas Street, received 9 March 2015
 14/006/P-352 B Material elevation - St Thomas Street, received 9 March 2015
 Archaeology evaluation, received 4 November 2014
 Design report minus appendices, received 4 November 2014
 Flood risk assessment, received 4 November 2014
 Sequential test report, received 4 November 2014
 Current flood zone plans (2009), received 4 November 2014
 Extract from BCC SWMP floor plans for Met Office warning levels, received 4 November 2014
 Future floor zone plans (undefended - 2060 and 2110), received 4 November 2014
 FRA sample section and plans, received 4 November 2014
 Undefended flood levels extract from CAFRA workstreams 3 and 4, received 4 November 2014
 Drainage drawings and calculations, received 4 November 2014

Development Control Committee B – 29 April 2015**Application No. 14/05459/F: 25 Redcliff Street 14/14A St Thomas Street Bristol BS1 6LR**

Transport statement, received 4 November 2014
 Travel plan 16.10.14, received 4 November 2014
 Ground investigation, received 4 November 2014
 Groundsure report and historic mapping, received 4 November 2014
 Groundsure Envirolnsight report, received 4 November 2014
 Groundsure report and historic mapping, received 4 November 2014
 Geology desk study and maps, received 4 November 2014
 Groundsure ground workings and map, received 4 November 2014
 Mining, extraction and natural cavities report and map, received 4 November 2014
 Natural ground subsidence and map, received 4 November 2014
 Borehole records, received 4 November 2014
 Estimated background soil chemistry, received 4 November 2014
 Railways and tunnels, received 4 November 2014
 Desk study contact details, received 4 November 2014
 Extract from CIRIA C552, received 4 November 2014
 Environmental permits, incidents and registers, received 4 November 2014
 Hydrogeology and hydrology, received 4 November 2014
 Designated environmentally sensitive sites map, received 4 November 2014
 Groundsure maps 1-4, received 4 November 2014
 Preliminary unexploded ordnance risk assessment, received 4 November 2014
 Air quality assessment, received 4 November 2014
 Structural sketches, received 4 November 2014
 CDM risk assessment, received 4 November 2014
 Drainage strategy, received 4 November 2014
 Energy strategy (Applied Energy), received 9 March 2015
 Adnitt Acoustics - 'External Noise Break -In Detailed Assessment', received 9 March 2015
 Health impact assessment, received 4 November 2014
 Heritage assessment, received 4 November 2014
 Landscape and visual impact assessment, received 4 November 2014
 Statement of community involvement, received 4 November 2014
 Viability assessment, received 10 March 2015
 Design and access statement, received 4 November 2014

Reason: For the avoidance of doubt.

Advices

1. Sound insulation/acoustic reports

The recommended design criteria for dwellings are as follows:

- * Daytime (07.00 - 23.00) 35 dB LAeq 16 hours in all rooms & 50 dB in outdoor living areas.
- * Nighttime (23.00 - 07.00) 30 dB LAeq 8 hours & LAm_{ax} less than 45 dB in bedrooms.

Where residential properties are likely to be affected by amplified music from neighbouring pubs or clubs, the recommended design criteria is as follows:

- * Noise Rating Curve NR20 at all times in any habitable rooms.

2. Details of extraction/ventilation system

It is recommended that any flues for the dispersal of cooking smells shall either:

- (a) Terminate at least 1 metre above the ridge height of any building in the vicinity, with no obstruction of upward movement of air or:

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(b) Have a method of odour control such as activated carbon filters, electrostatic precipitation or inline oxidation.

Guidance on the above can be gained at 'Guidance on the Control of Odour & Noise from Commercial Kitchen Exhaust Systems' available from www.defra.gov.uk by searching for Product Code PB10527.

3. Odour Management Plan

Guidance on the above can be gained at 'Guidance on the Control of Odour & Noise from Commercial Kitchen Exhaust System' Published electronically by Department for Environment, Food and Rural Affairs. Product Code PB10527. <http://www.defra.gov.uk/environment/noise/research/kitchenexhaust/pdf/kitchenreport.pdf> And 'Odour Guidance for Local Authorities' Published electronically by Department for Environment, Food and Rural Affairs. <http://www.defra.gov.uk/environment/quality/local/nuisance/odour/documents/local-auth-guidance.pdf>

4. Internal nest trays or boxes are particularly recommended for swifts. Swift bricks are best provided in pairs or groups (e.g. two or three on a building, avoiding windows) at least one metre apart. This is because they are usually colonial nesters. Swift boxes/bricks are best located on north or east facing walls, at least 5 metres high, so that there is a clear distance (drop) below the swift boxes/bricks of 5 metres or more so that there is space for the swifts to easily fly in and out of the boxes. One of the best designs is those by Schwegler because they are very durable.
5. All species of bats and their roosts are legally protected. If bats are encountered all demolition or construction work should cease and the Bat Conservation Trust (Tel 0845 1300 228) should be consulted for advice.
6. The applicant is advised to be aware of the possibility of birds nesting on or in buildings on site. All species of wild birds, their eggs, nests and chicks are legally protected until the young have fledged. The bird nesting season typically runs between 1st March and 30th September inclusive in any year. If birds are found to be nesting on buildings a qualified ecological consultant should be contacted immediately for further advice.
7. Please note that the Crime Prevention Design Advisor has recommended that the applicant applies for Secured by Design. Secured by Design is a minimum standard for security, the applicant should familiarise themselves with this site www.securedbydesign.com to obtain site specific information.
8. Right of light: The buildings that you propose may affect a right of light enjoyed by the neighbouring property. This is a private right which can be acquired by prescriptive uses over 20 years; as such it is not affected in any way by the grant of planning permission.
9. Oversailing: Where new structures overhang or cross a highway an oversailing agreement must be obtained from the Local Highway Authority before any works commence. (Telephone 0117 9222100).
10. Works on the public highway: The development hereby approved includes the carrying out of work on the public highway. You are advised that before undertaking work on the highway you must enter into a formal agreement with the council which would specify the works and the terms and conditions under which they are to be carried out. You should contact City Development, Wilder House, Wilder Street, Bristol, BS2 8PH or telephone 0117 903 6846, allowing sufficient time for the preparation and signing of the agreement. You will be required

Development Control Committee B – 29 April 2015**Application No. 14/05459/F: 25 Redcliff Street 14/14A St Thomas Street Bristol BS1 6LR**

to pay fees to cover the councils cost's in undertaking the following actions:

- 1) Drafting the agreement
 - 2) A monitoring fee equivalent to 15% of the planning application fee
 - 3) Approving the highway details
 - 4) Inspecting the highway works.
11. Traffic Regulation Order (TRO): In order to comply with the requirements of condition; you are advised that the implementation of a TRO is required. The TRO process is a lengthy legal process involving statutory public consultation and you should allow an average of 6 months from instruction to implementation. You are advised that the TRO process cannot commence until payment of the TRO fees are received. Telephone 0117 9036846 to start the TRO process.
 12. The development hereby approved is likely to impact on the highway network during its construction. The applicant is required to contact Highway Network Management to discuss any temporary traffic management measures required, such as footway, Public Right of Way or carriageway closures, or temporary parking restrictions. Please call 0117 9031212 or email traffic@bristol.gov.uk a minimum of eight weeks prior to any activity on site to enable Temporary Traffic Regulation Orders to be prepared and a programme of Temporary Traffic Management measures to be agreed.
 13. Note that in deciding to grant permission, the Committee/Planning Service Director also decided to recommend to the Council's Executive in its capacity as Traffic Authority in the administration of the existing Controlled Parking Zone of which the development forms part, that the development should be treated as car free / low-car and the occupiers ineligible for resident parking permits.
 14. Please note, the final detailed specification of the photovoltaic system should be prepared by a qualified installer and should comprise the manufacturers technical data and, applying the MCS standard estimation method, a calculation of estimated annual electricity generated.

BACKGROUND PAPERS

Air Quality	10 December 2014
Contaminated Land Environmental Protection	22 December 2014
Crime Reduction Unit	10 December 2014
Flood Risk Manager	10 December 2014
Historic England	9 December 2014
Environment Agency (Sustainable Places)	2 February 2015
	11 March 2015

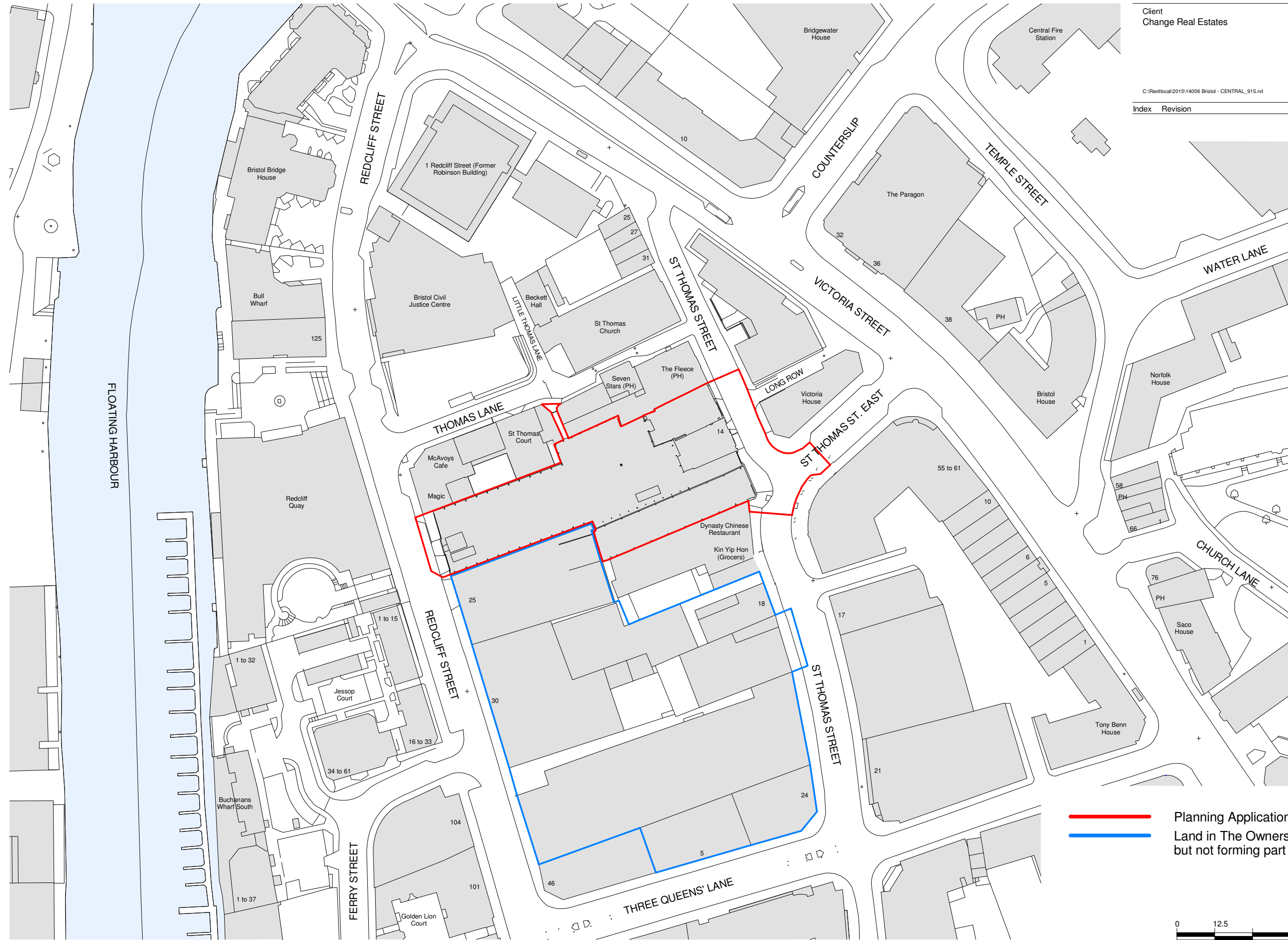
Supporting Documents

2. **25 Redcliff Street, 14/14A St Thomas Street.**
 1. Site location plan.
 2. Existing site plan.
 3. Proposed site plan.
 4. Proposed ground floor plan.
 5. Proposed first floor plan.
 6. Proposed second floor plan.
 7. Proposed third floor plan.
 8. Proposed fourth floor plan.
 9. Proposed fifth floor plan.
 10. Proposed sixth floor plan.
 11. Proposed seventh floor plan.
 12. Proposed roof plan.
 13. Existing & proposed contextual sections.
 14. Proposed long sections – sheet 1.
 15. Proposed long sections – sheet 2.
 16. Proposed cross sections.
 17. Existing & proposed Redcliff Street elevation.
 18. Existing & proposed St Thomas Street elevation.
 19. Proposed block A internal elevations.
 20. Proposed block B internal elevation.
 21. Existing & proposed north elevation.
 22. Existing & proposed internal north elevation.
 23. Material elevation – Redcliff Street.
 24. Material elevation – St Thomas Street.
 25. Material elevation – internal courtyards.
 26. Illustrative view – Proposed Redcliff Street frontage.
 27. Illustrative view – Proposed St Thomas Street.
 28. Illustrative view – Proposed internal courtyard.

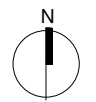
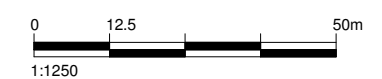
Client
Change Real Estates

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Index	Revision	Date	Int	Chkd



— Planning Application Boundary
— Land in The Ownership of The Applicant but not forming part of the application



APPROXIMATE NORTH

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Nero Brewery, Cricket Green, Hartley Wintney, Hants RG27 8QA
Tel: 01252 844144 Fax: 01252 844800 Web: www.lsharch.co.uk

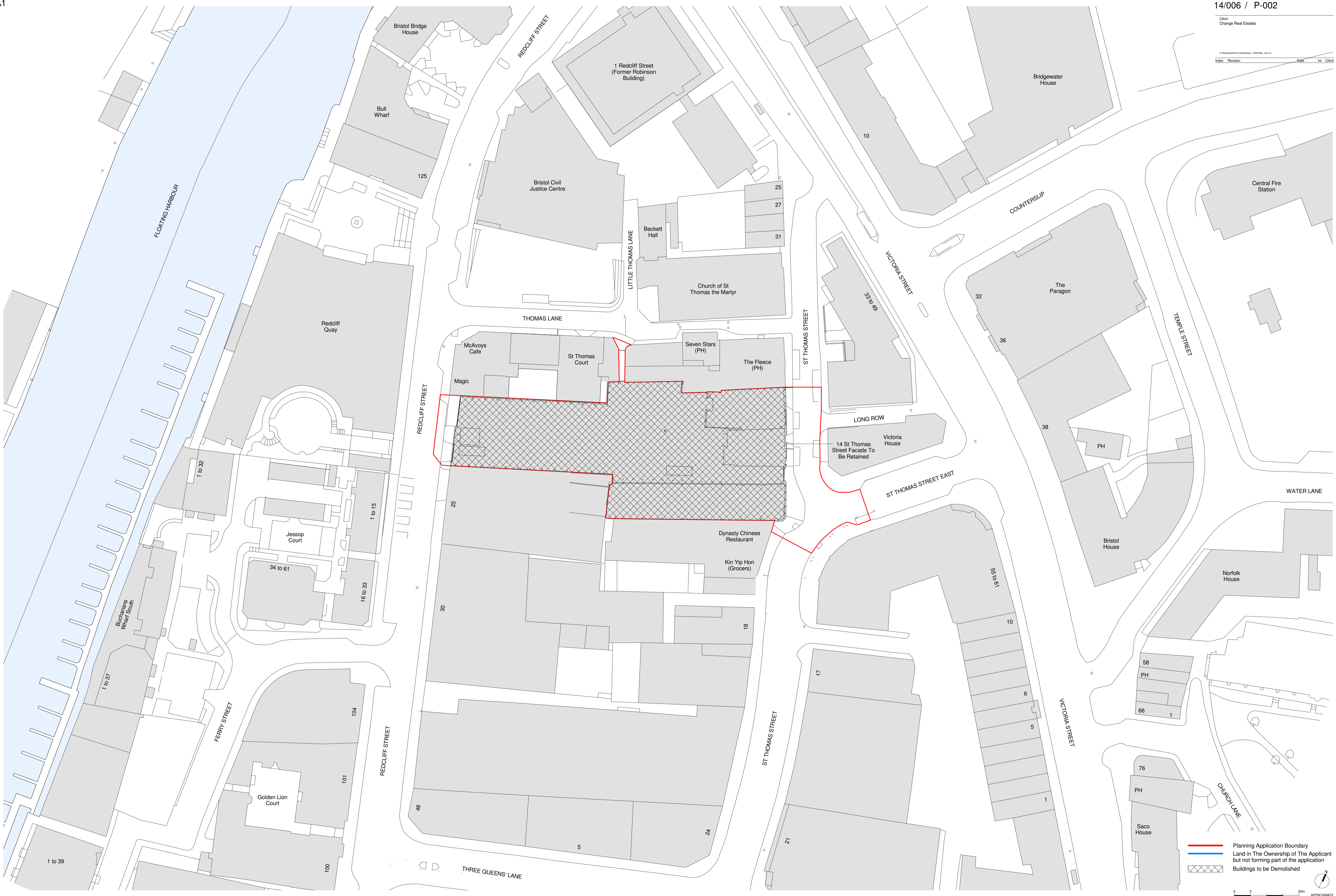
Job N Drg N 14/006 / P-001	Rev B	Scale As indicated	Status PLANNING
Date 12/09/2014	Director DH	Author AH	Check RW

Job
SAMPSON COURT, BRISTOL

Drawing
SITE LOCATION PLAN

NOTE: All figures are approximate and have been measured and expressed in a manner as defined by the current edition of the RICS/ISVA Code of Measuring Practice. Figures relate to the current stage of the project and any development decisions to be made on the basis of this information should include due allowance for the increases and decreases inherent in the design and building processes. Reproduced from Ordnance Survey mapping under Licence Numbers AR152978 & 100020449

This drawing may be scaled or cross referenced to the scale bar for Planning Application purposes only. Do not scale for any other purpose, use figured dimensions only. Subject to site survey and all necessary consents. All dimensions to be checked by user and any discrepancies, errors or omissions to be reported to the Architect before work commences. This drawing is to be read in conjunction with all other relevant materials.



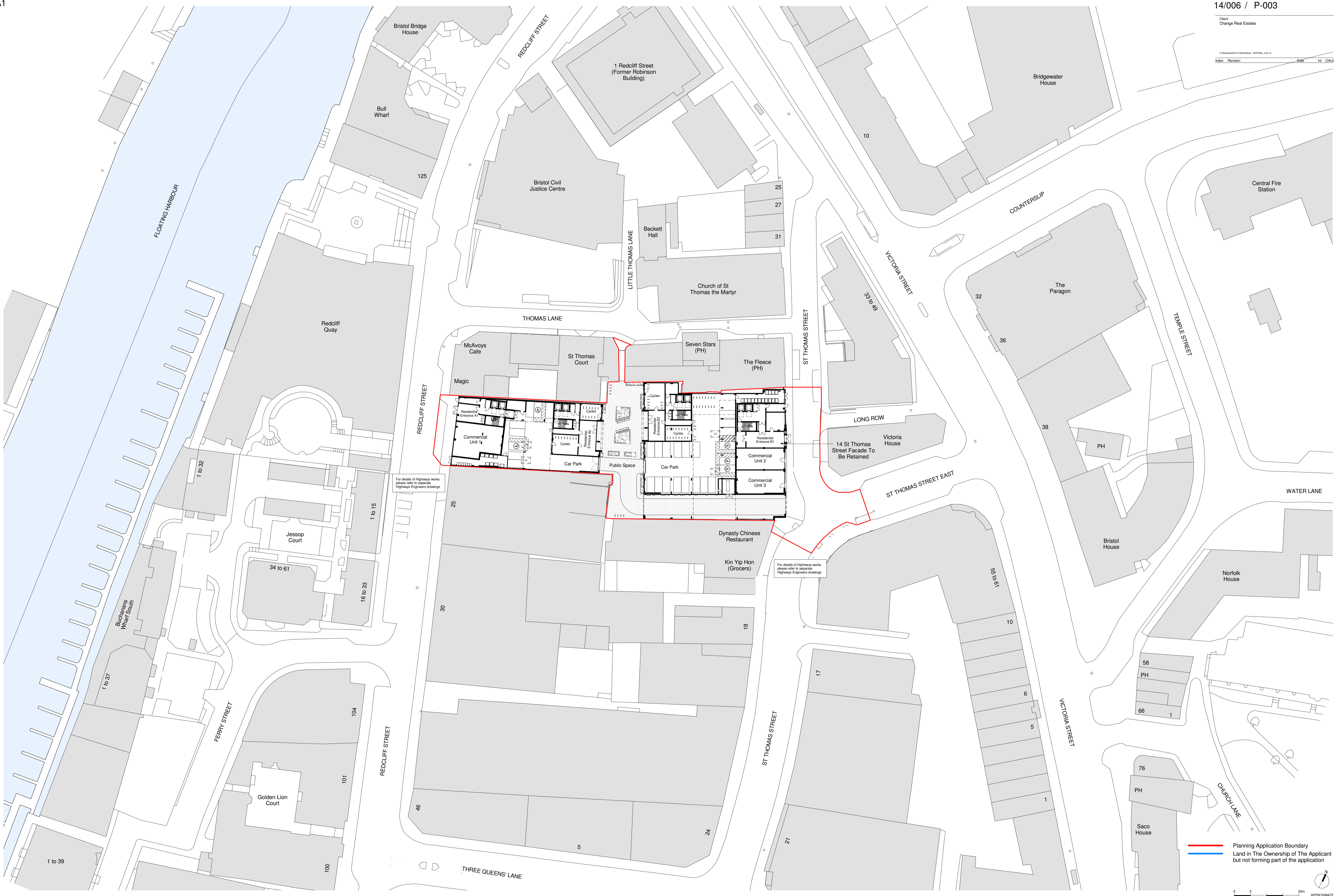
— Planning Application Boundary
— Land in The Ownership of The Applicant but not forming part of the application
 Buildings to be Demolished

0 5 20m
 1:500 APPROXIMATE NORTH

SAMPSON COURT, BRISTOL

Job N. Dwg N.	Rev	Scale	Status	Job
14/006 / P-002	B	As indicated	PLANNING	SAMPSON COURT, BRISTOL
Date	Director	Author	Check	Drawing
12/09/2014	DH	AH	RW	EXISTING SITE PLAN

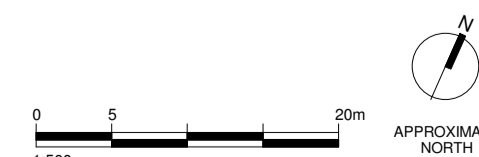
NOTE: All figures are approximate and have been measured and expressed as a minor as defined by the current edition of the BRISTOL City Council's Measuring Practice. Figures refer to the current stage of the project and any development decisions to be made on the basis of the relevant local planning authority. For the avoidance of doubt, the applicant is responsible for the design and build of the proposed development. This drawing is to be read in conjunction with all other relevant materials.



For details of Highways works please refer to separate Highways Engineers drawings

For details of Highways works please refer to separate Highways Engineers drawings

— Planning Application Boundary
 — Land in The Ownership of The Applicant but not forming part of the application



SAMPSON COURT, BRISTOL

THOMAS LANE

Church of St Thomas the Martyr

Seven Stars (PH)

The Fleece (PH)

St Thomas Court

McAvoy's Cafe

For details of Highways works please refer to separate Highways Engineers drawings

+8.28

ST THOMAS STREET

14 St Thomas Street Refurbished Facade

+8.21

+8.28

+8.34

+8.40

+8.48

For details of Highways works please refer to separate Highways Engineers drawings

REDCLIFF STREET

Car Club Parking

+8.79

+8.75

+8.74

+8.75

+8.68

+8.98

+8.52



Landscape proposals are illustrative and subject to detailed design

Visitor Cycle Parking (8)

Dashed line indicates extent of buildings above

Secure Entrance Gates

20 Private Car Parking Spaces

8 Private Car Parking Spaces

Residential Entrance A2 +8.495

MOE

Core 2 +8.495

Commercial Unit 1 +8.775

Residential Entrance A1 +8.775

MOE

Core 1 +8.775

Water Tank

Residential Entrance B2 +8.495

Core 3 +8.495

Secure Cycle Parking (22)

Residential Entrance B1 +8.240

Core 4 +8.240

Secure Cycle Parking (30)

MOE

Core 3 +8.495

Secure Cycle Parking (32)

Motorcycles

Bin Store B +8.242

Bin Store A

MOE

Core 4 +8.240

Residential Entrance B1 +8.240

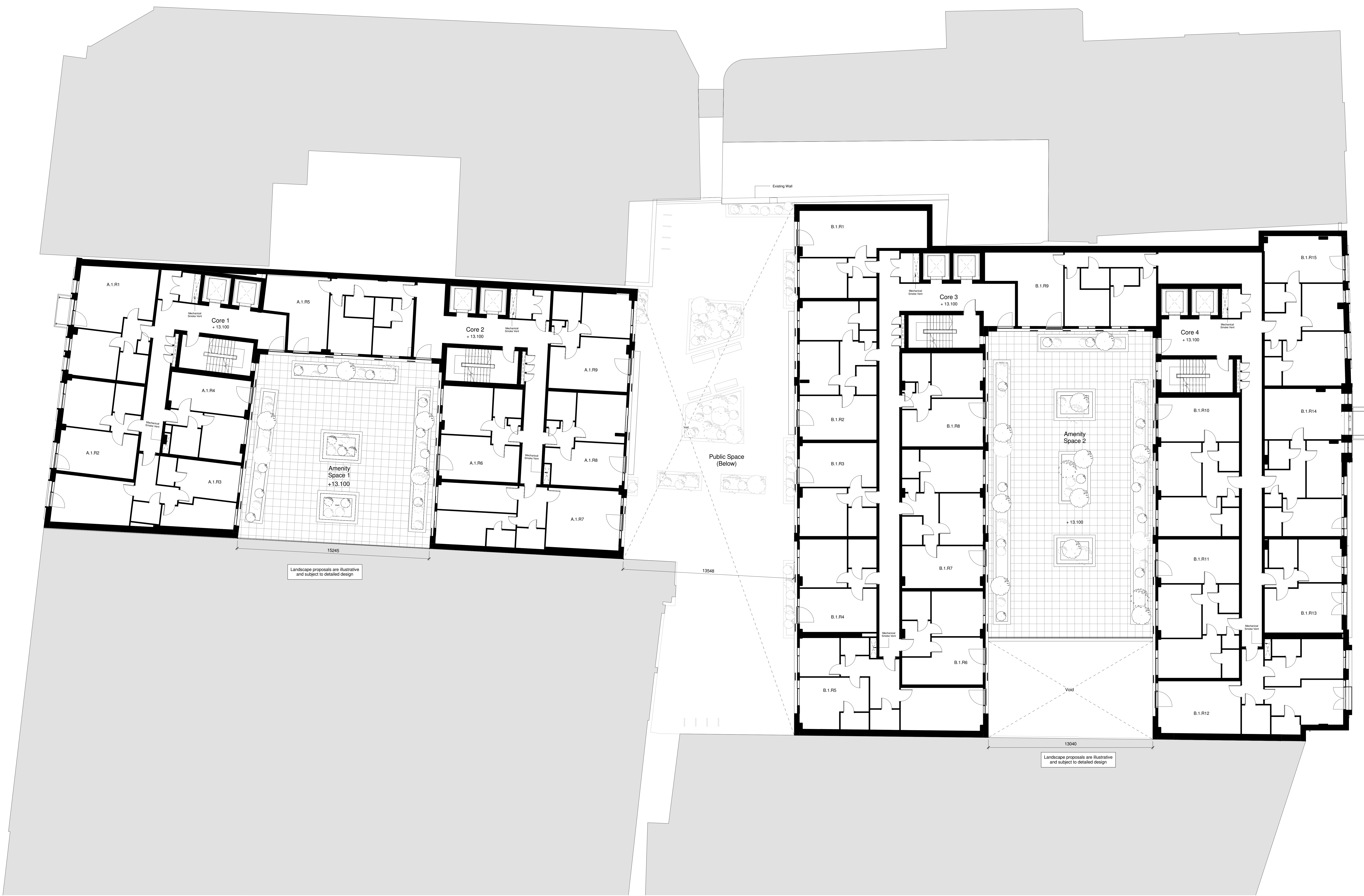
Commercial Unit 2 +8.250

Commercial Unit 3 +8.250

Electricity Substation

Dynasty Chinese Restaurant

Kin Yip Hon (Grocers)



SAMPSON COURT, BRISTOL



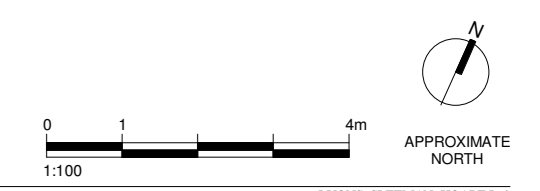




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Landscape proposals are illustrative and subject to detailed design



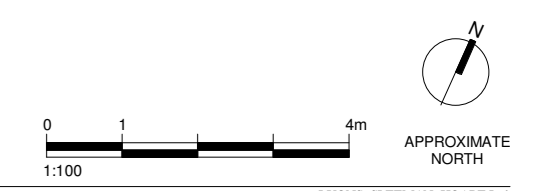
SAMPSON COURT, BRISTOL



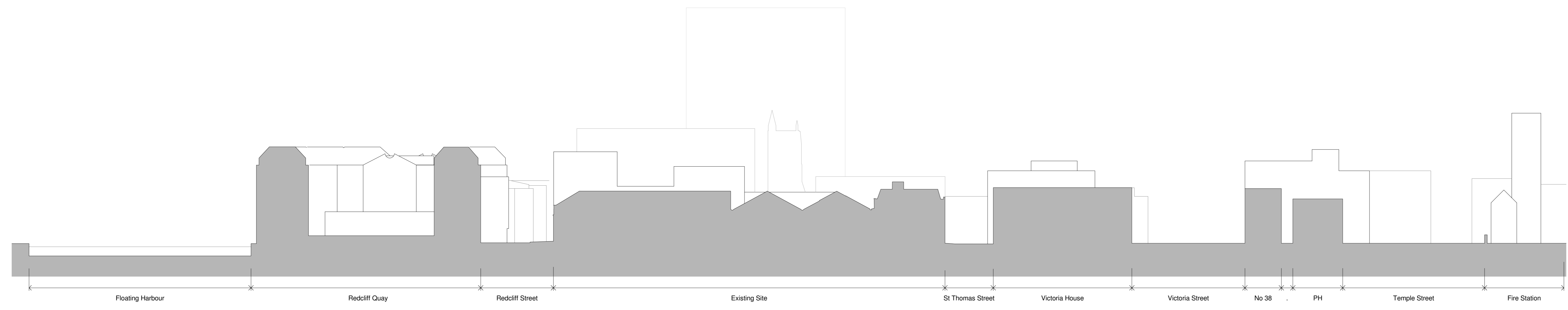
SAMPSON COURT, BRISTOL



Landscape proposals are illustrative and subject to detailed design



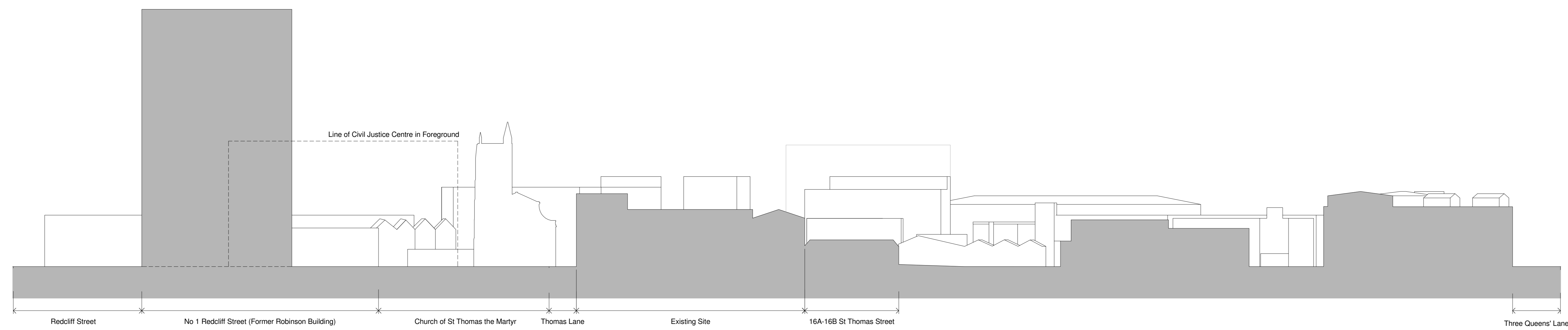




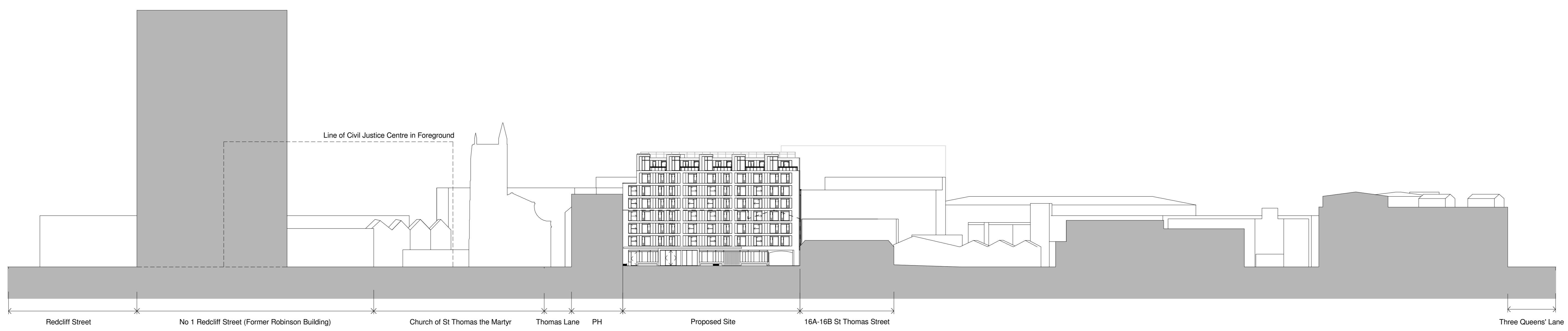
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2 | PROPOSED CONTEXTUAL SECTION AA
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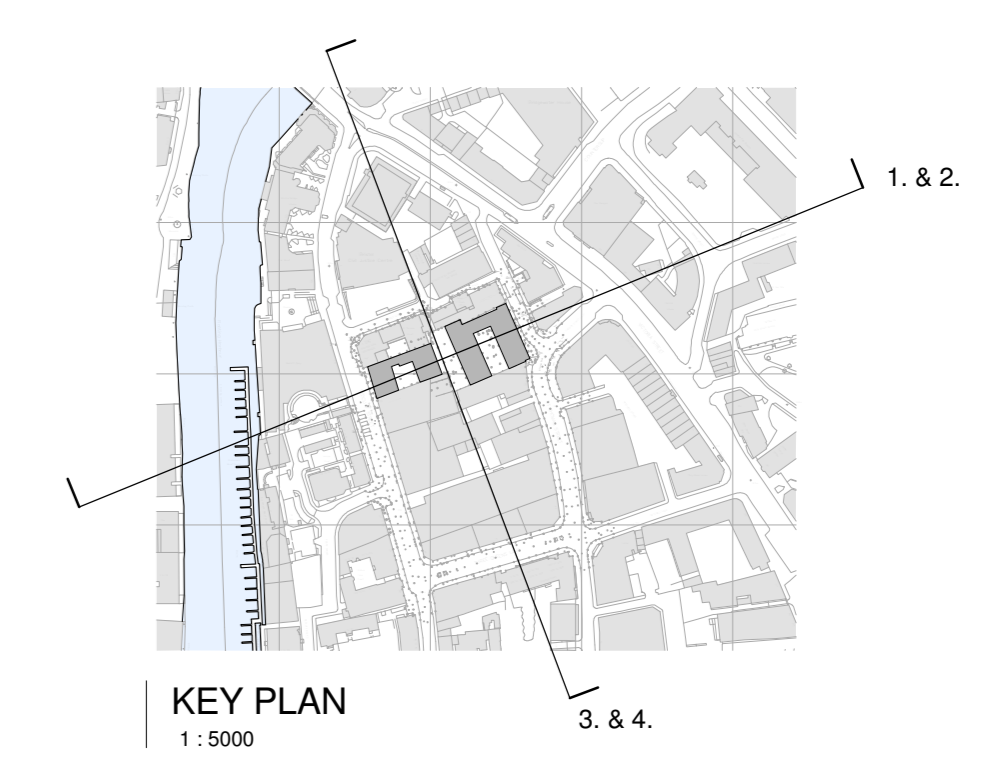


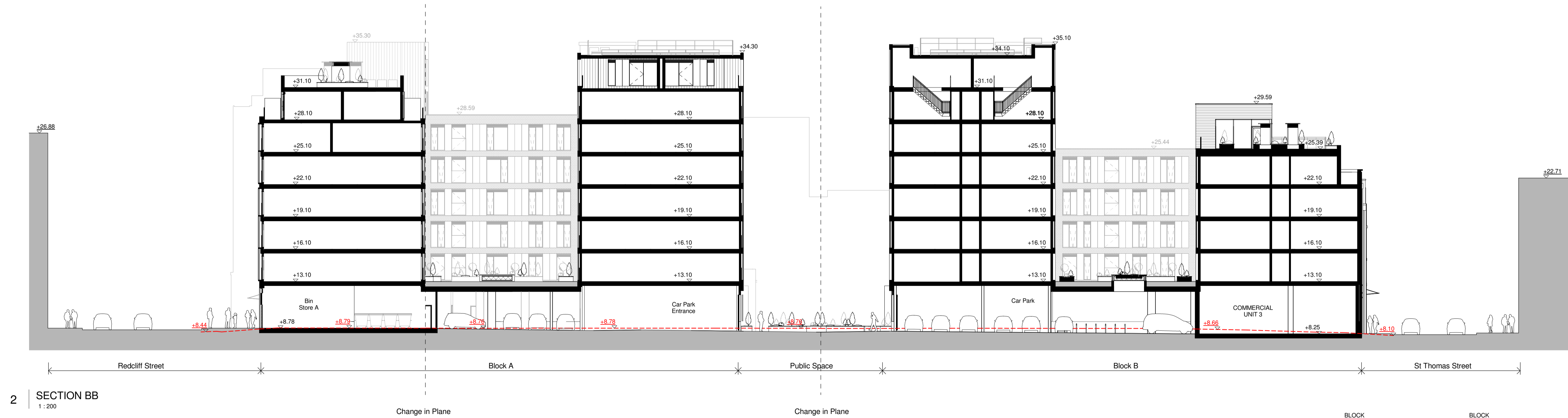
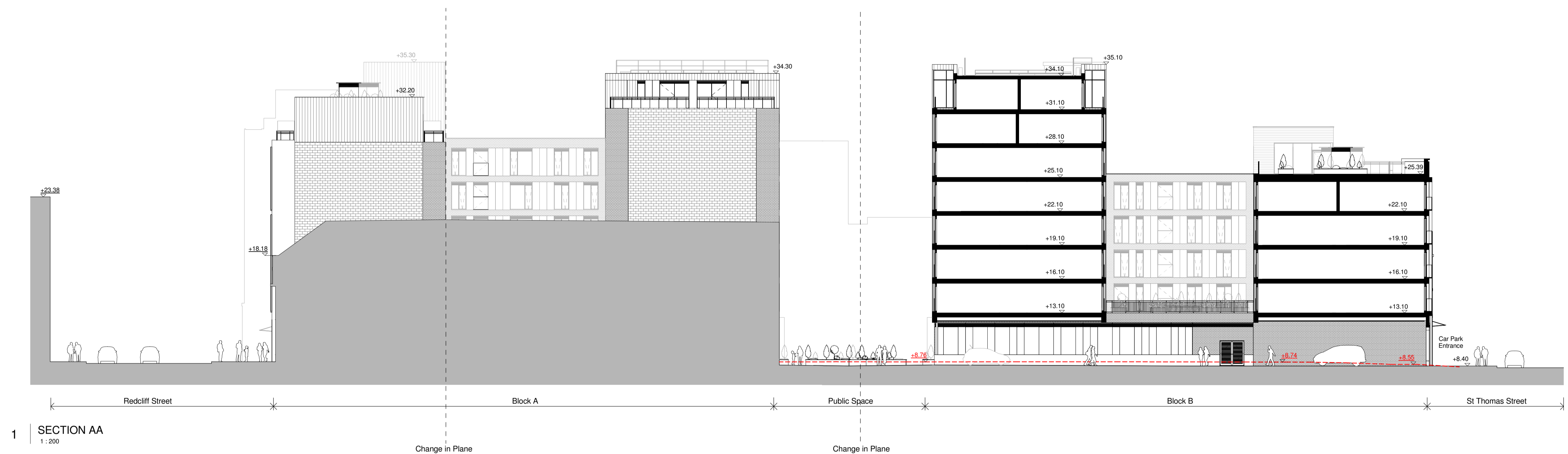
3 | PROPOSED CONTEXTUAL SECTION BB
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4 | PROPOSED CONTEXTUAL SECTION BB
 1:500

Legend
 --- Existing drawings based on Sumo Survey Services drawing 140776 02 A
 --- Building heights are generally indicative. Confirmed heights are indicated with level marker.
 --- Existing building levels are based on Sumo Survey Services drawing 140776 02 A

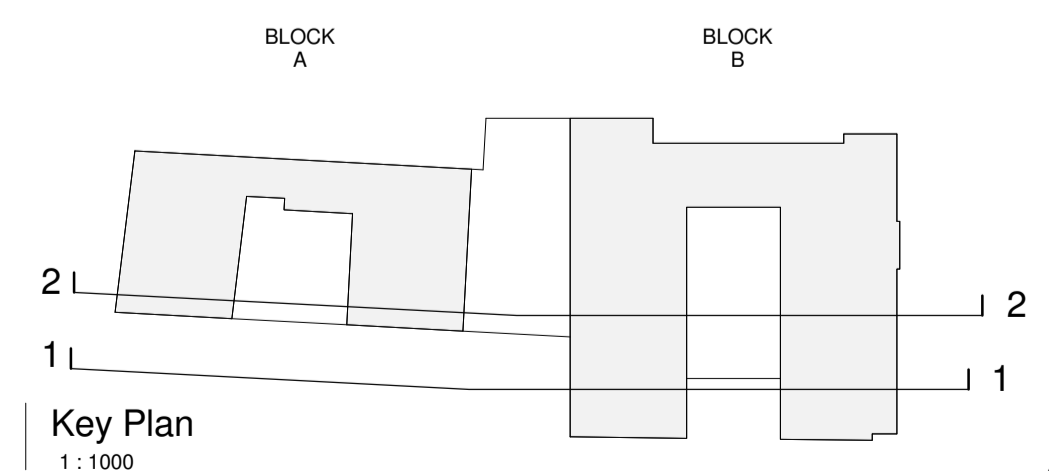




Legend

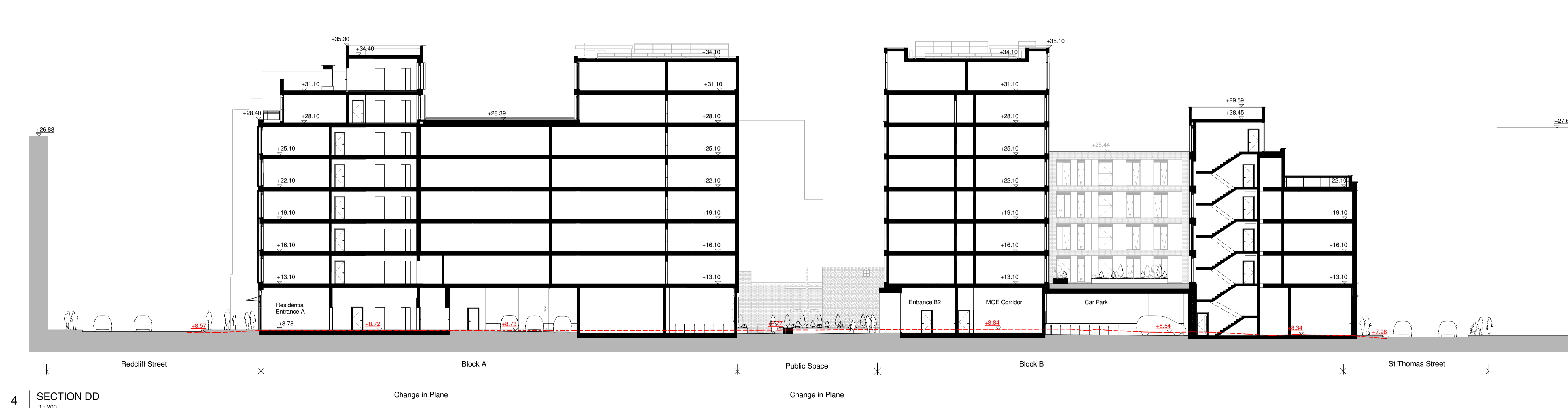
- - - Existing line of Topographical Survey based on Sumo Survey Services drawing 140776 01 A
- +0.00 Proposed Level
- Existing Level
- +0.00 Existing Surface Level

NOTE:
- Building heights are generally indicative. Confirmed heights are indicated with level marker.
- Existing building levels are based on Sumo Survey Services drawing 140776 02 A





3 SECTION CC
1:200



4 SECTION DD
1:200

Legend

--- Existing line of Topographical Survey based on Sumo Survey Services drawing 140776 01 A

+0.00 Proposed Level

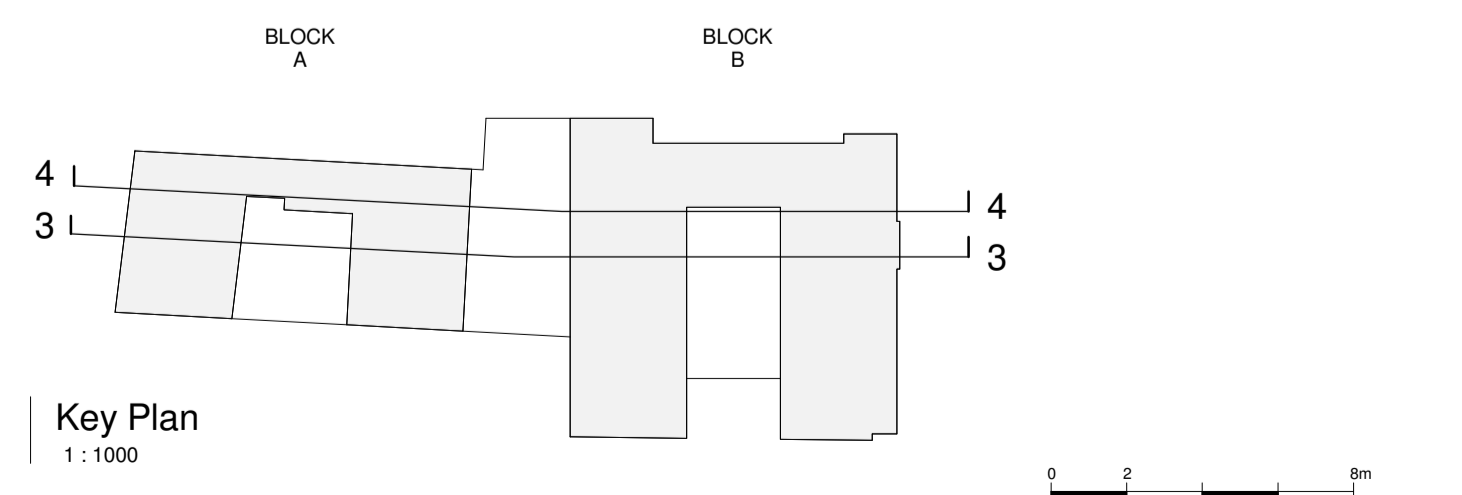
±0.00 Existing Level

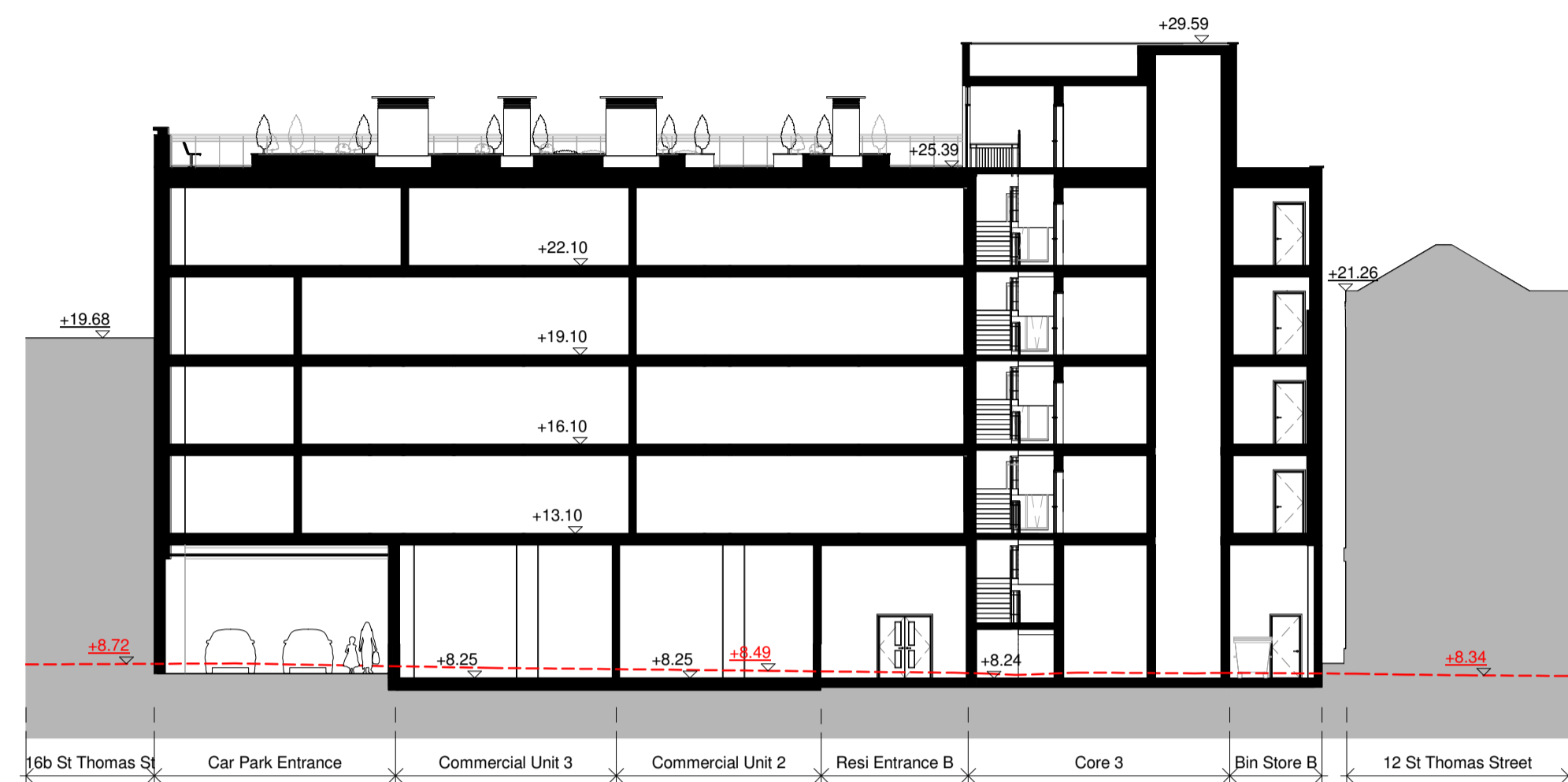
+0.00 Existing Surface Level

NOTE:

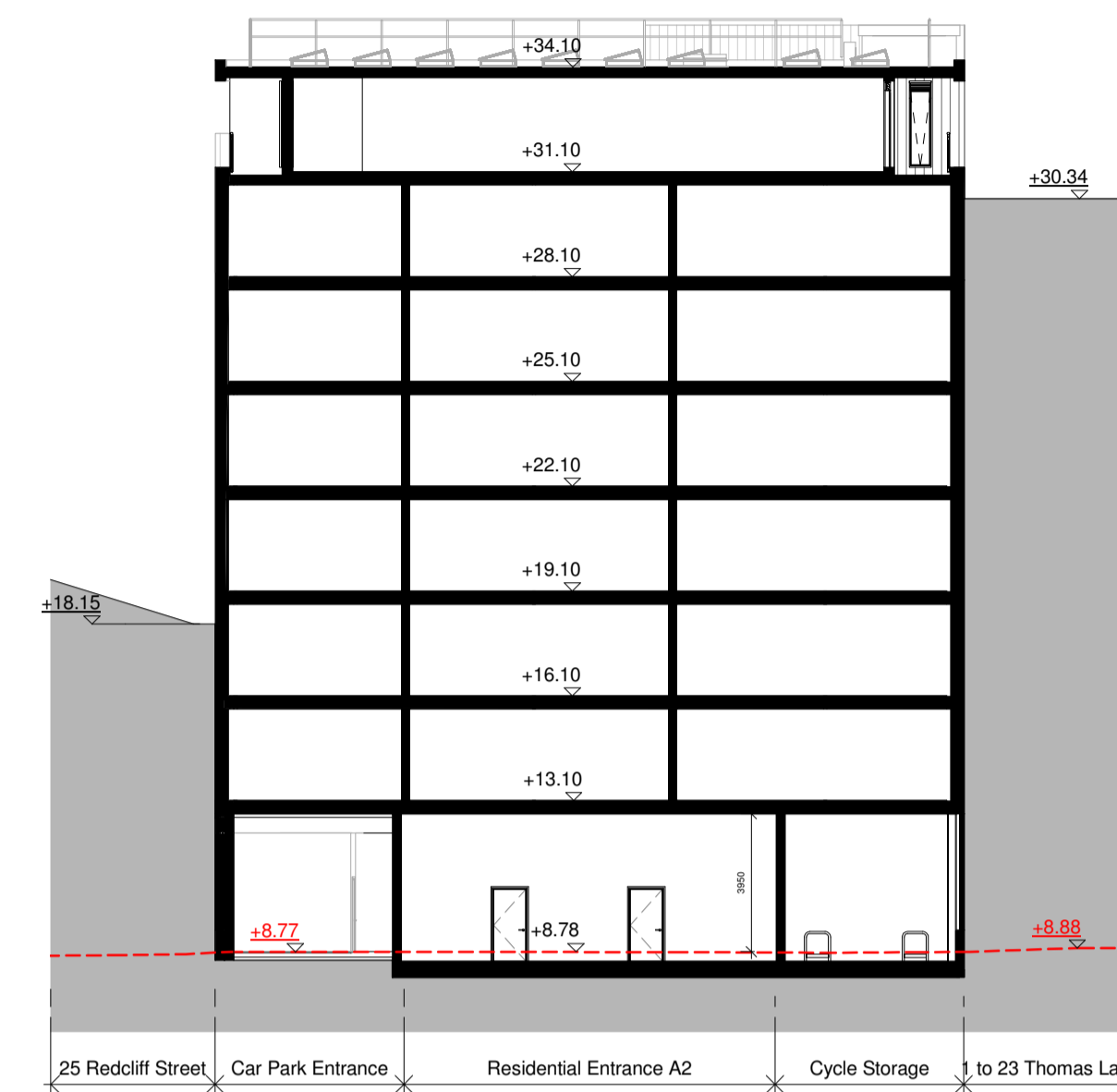
- Building heights are generally indicative. Confirmed heights are indicated with level marker.

- Existing building levels are based on Sumo Survey Services drawing 140776 02 A





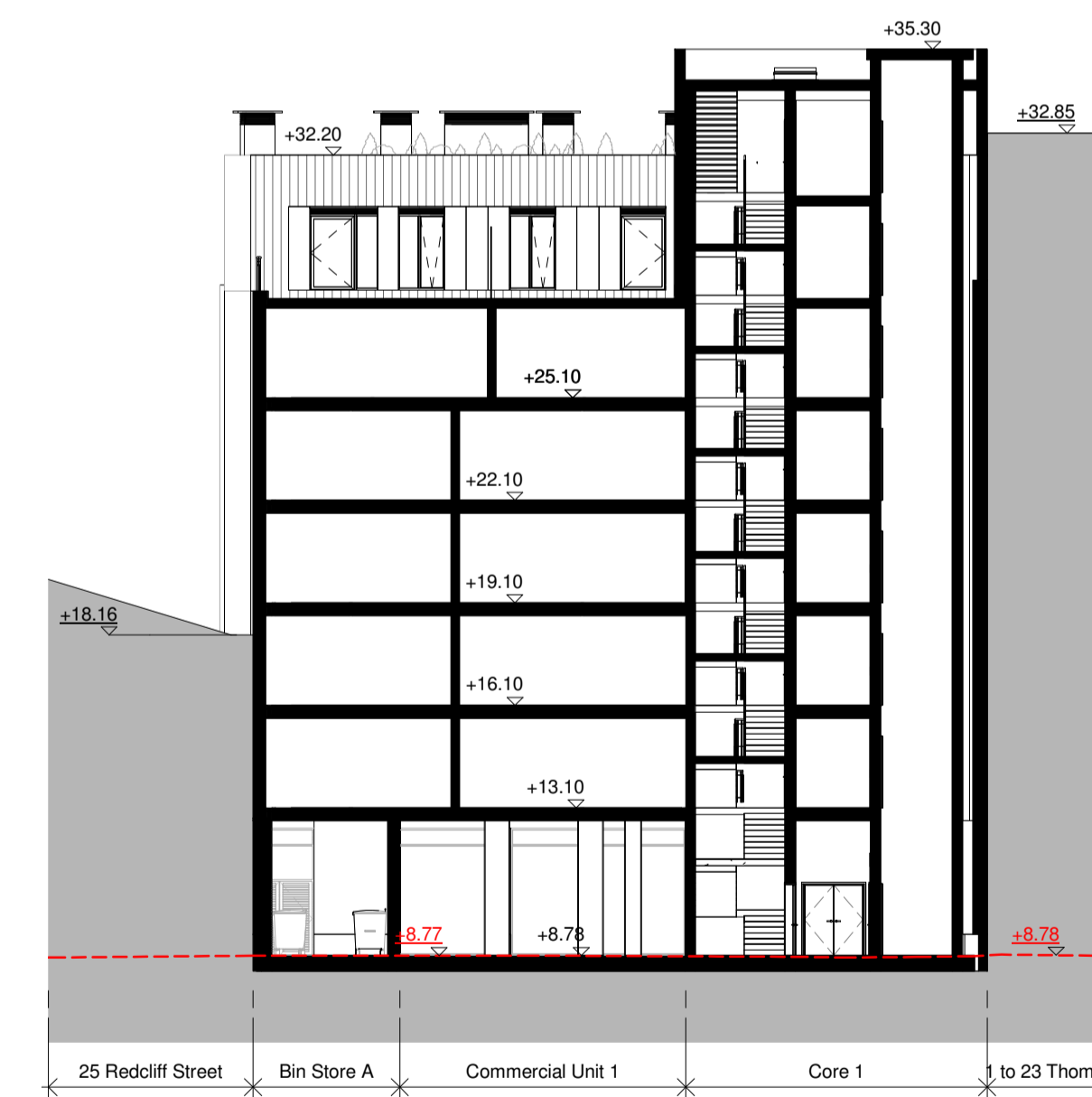
5 BLOCK B - SECTION EE
1 : 200



7 BLOCK A - SECTION GG
1 : 200



6 BLOCK B - SECTION FF
1 : 200



8 BLOCK A - SECTION HH
1 : 200

Legend

--- Existing line of Topographical Survey based on Sumo Survey Services drawing 140776 01 A

+0.00 Proposed Level

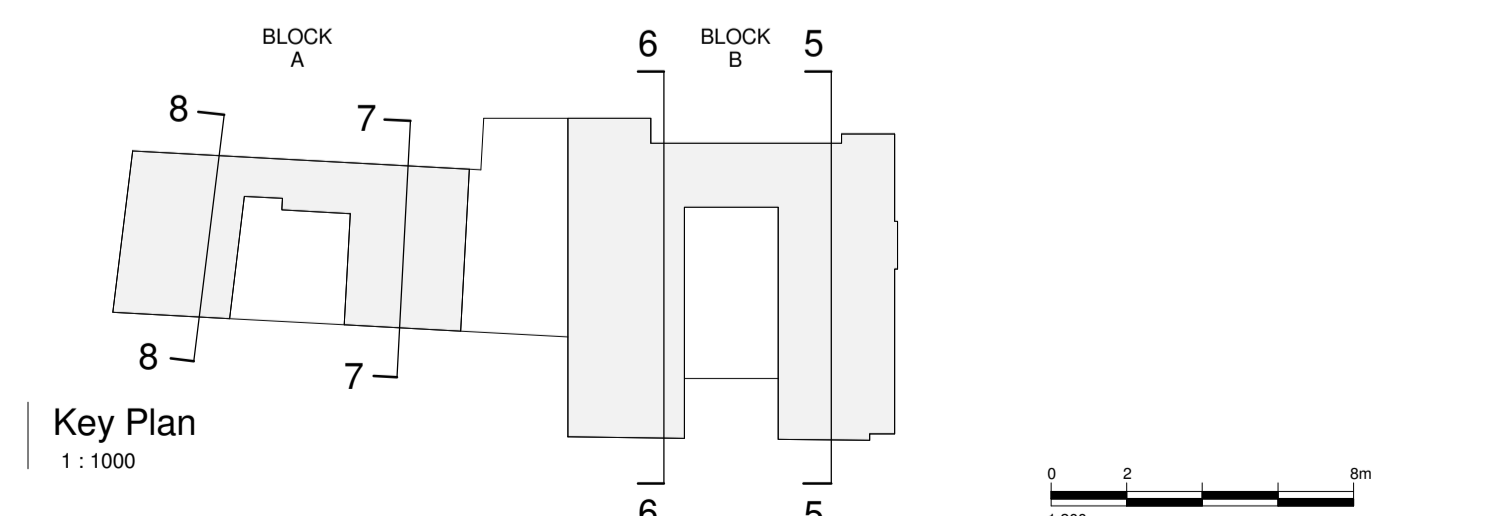
▽ Existing Level

±0.00 Existing Level

+0.00 Existing Surface Level

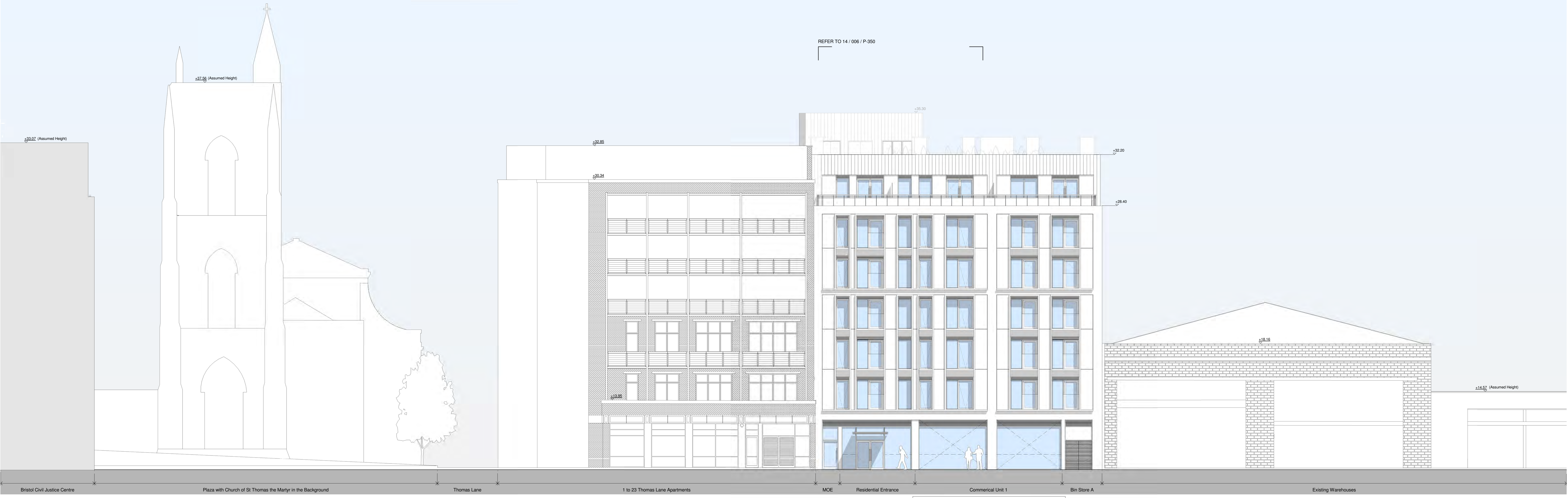
NOTE:

- Building heights are generally indicative. Confirmed heights are indicated with level marker.
- Existing building levels are based on Sumo Survey Services drawing 140776 02 A





1 | EXISTING REDCLIFF STREET ELEVATION
1 : 100



2 | PROPOSED REDCLIFF STREET ELEVATION
1 : 100

Legend
Existing building levels are based on Sumo Survey Services drawing 140778-02 A
Existing building materials are illustrative only and are based on Sumo Survey Services drawing 140778-02 A
+0.00 Proposed Level
-0.00 Existing Level



1 | EXISTING ST THOMAS STREET ELEVATION
1:100



2 | PROPOSED ST THOMAS STREET ELEVATION
1:100

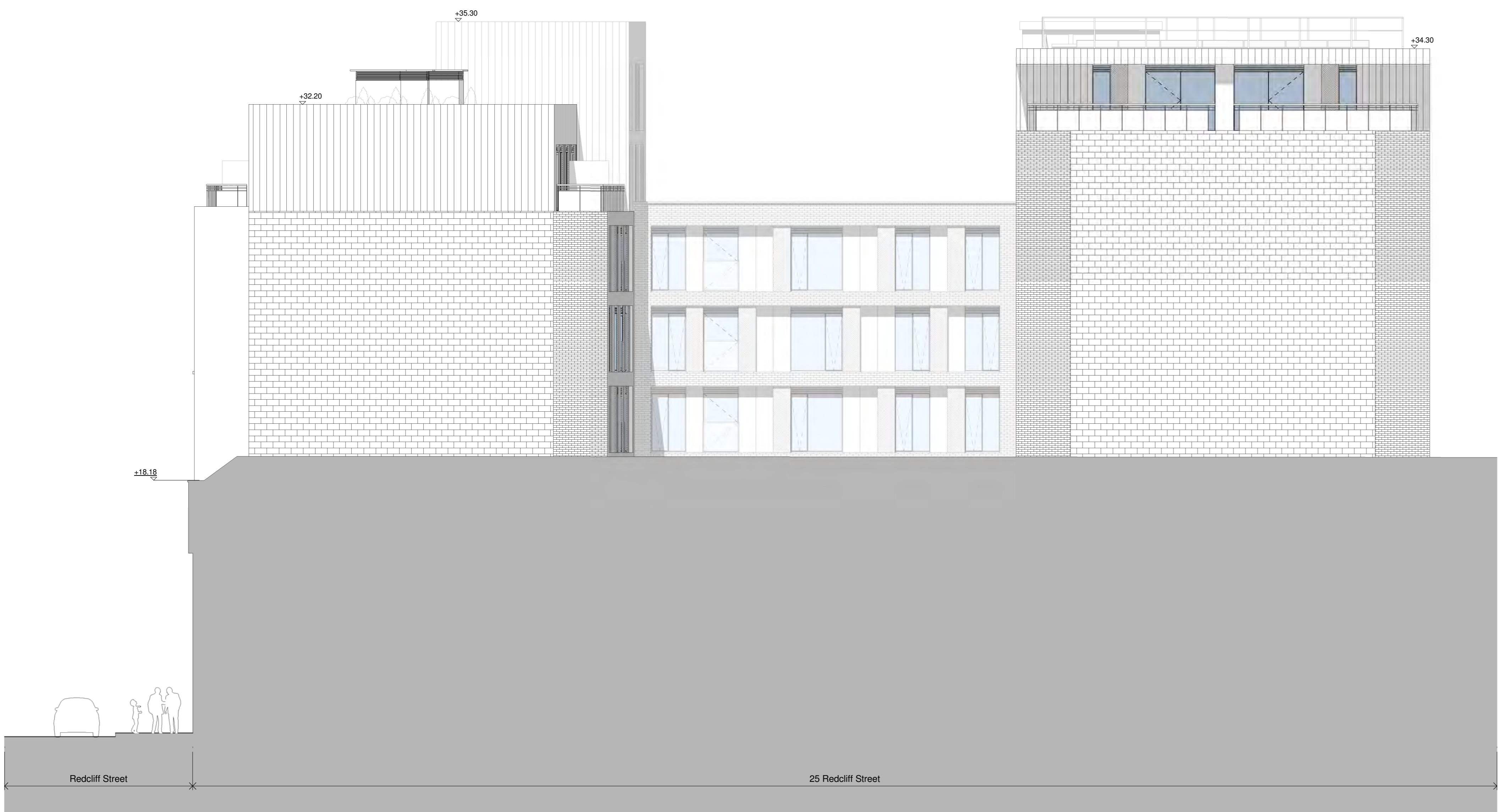
Legend

Existing building levels are based on Sumo Survey Services drawing 140776 02 A

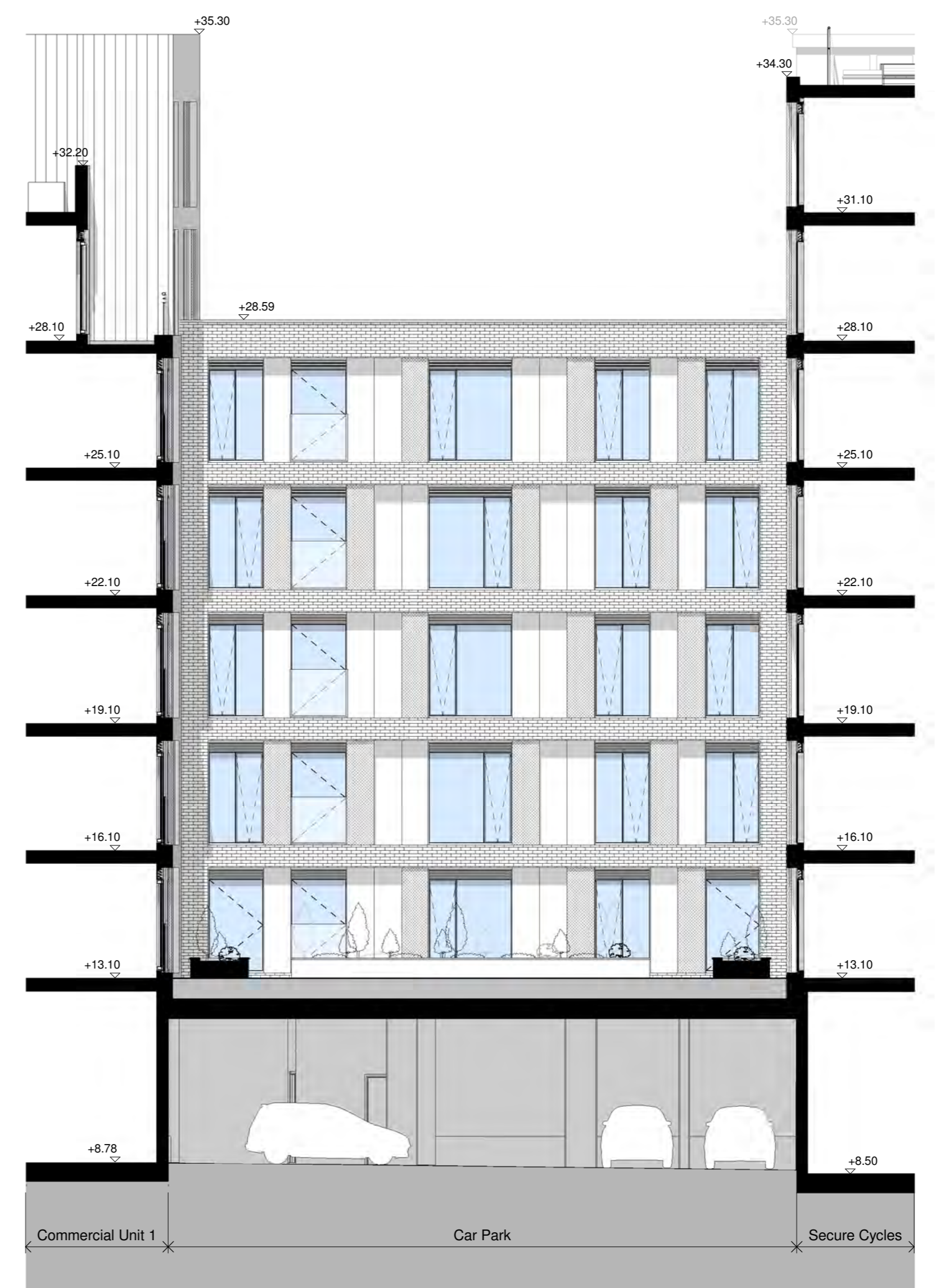
Existing building materials are illustrative only and are based on Sumo Survey Services drawing 140776 02 A

+0.00 Proposed Level

+0.00 Existing Level



1 BLOCK A - SOUTH ELEVATION
1:100



2 BLOCK A - AMENITY SPACE 1 - SOUTH ELEVATION
1:100



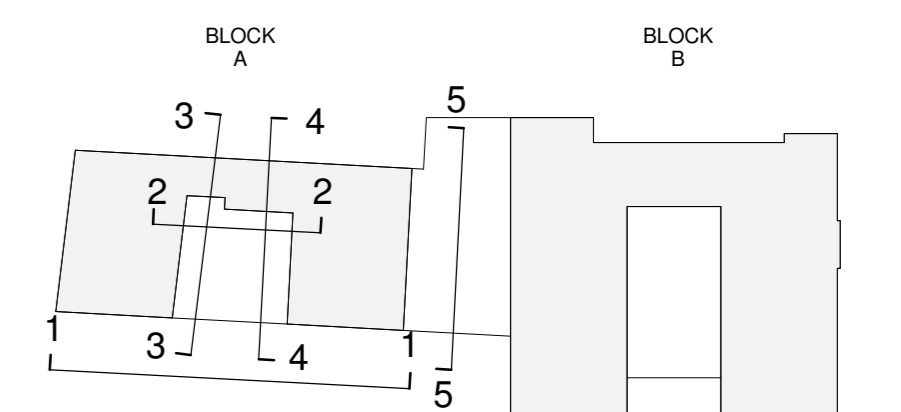
3 BLOCK A - AMENITY SPACE 1 - EAST ELEVATION
1:100



4 BLOCK A - AMENITY SPACE 1 - WEST ELEVATION
1:100



5 BLOCK A - EAST ELEVATION
1:100



Key Plan
1:1000

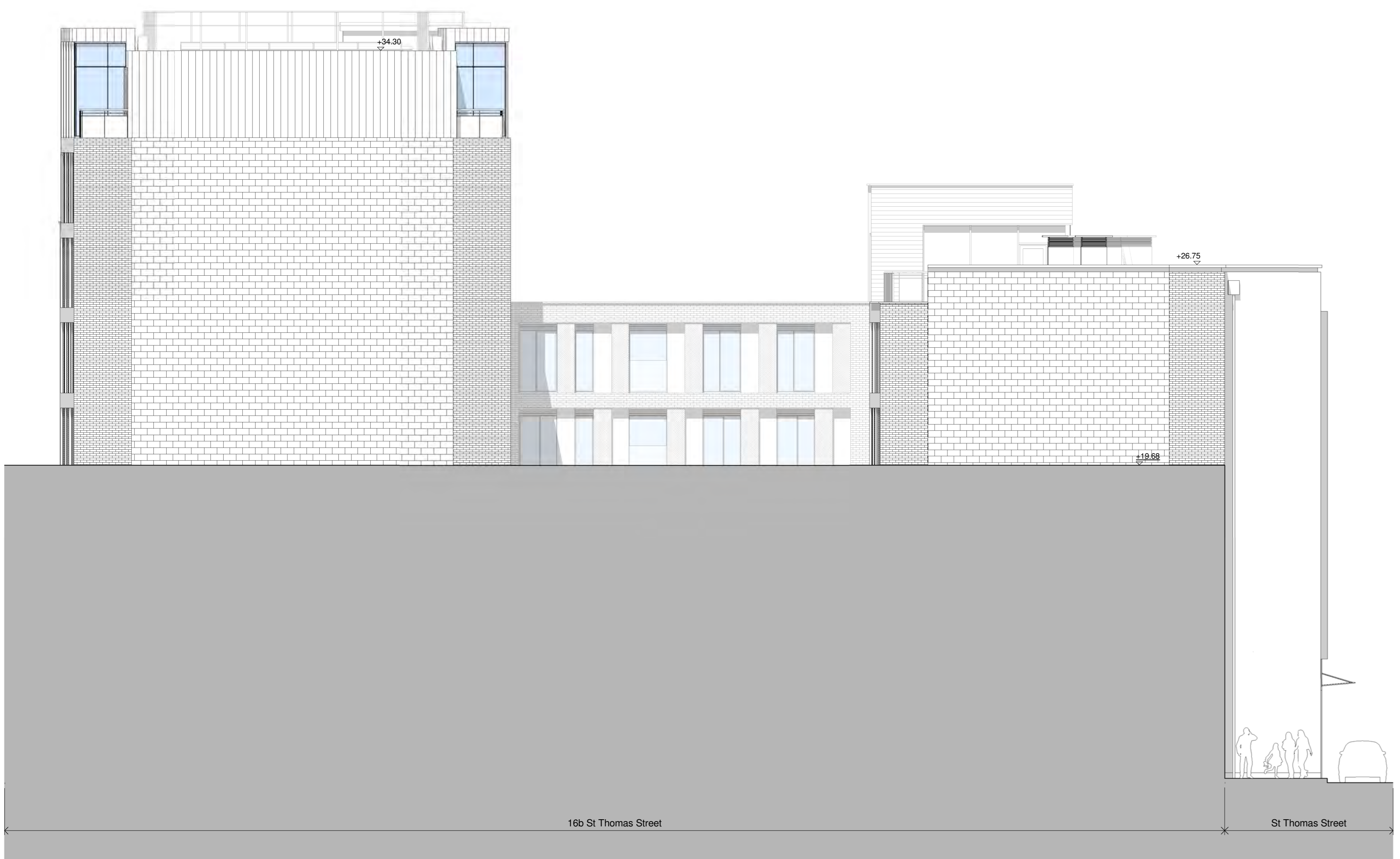
Legend

Existing building levels are based on Sumo Survey Services drawing 140776 02 A

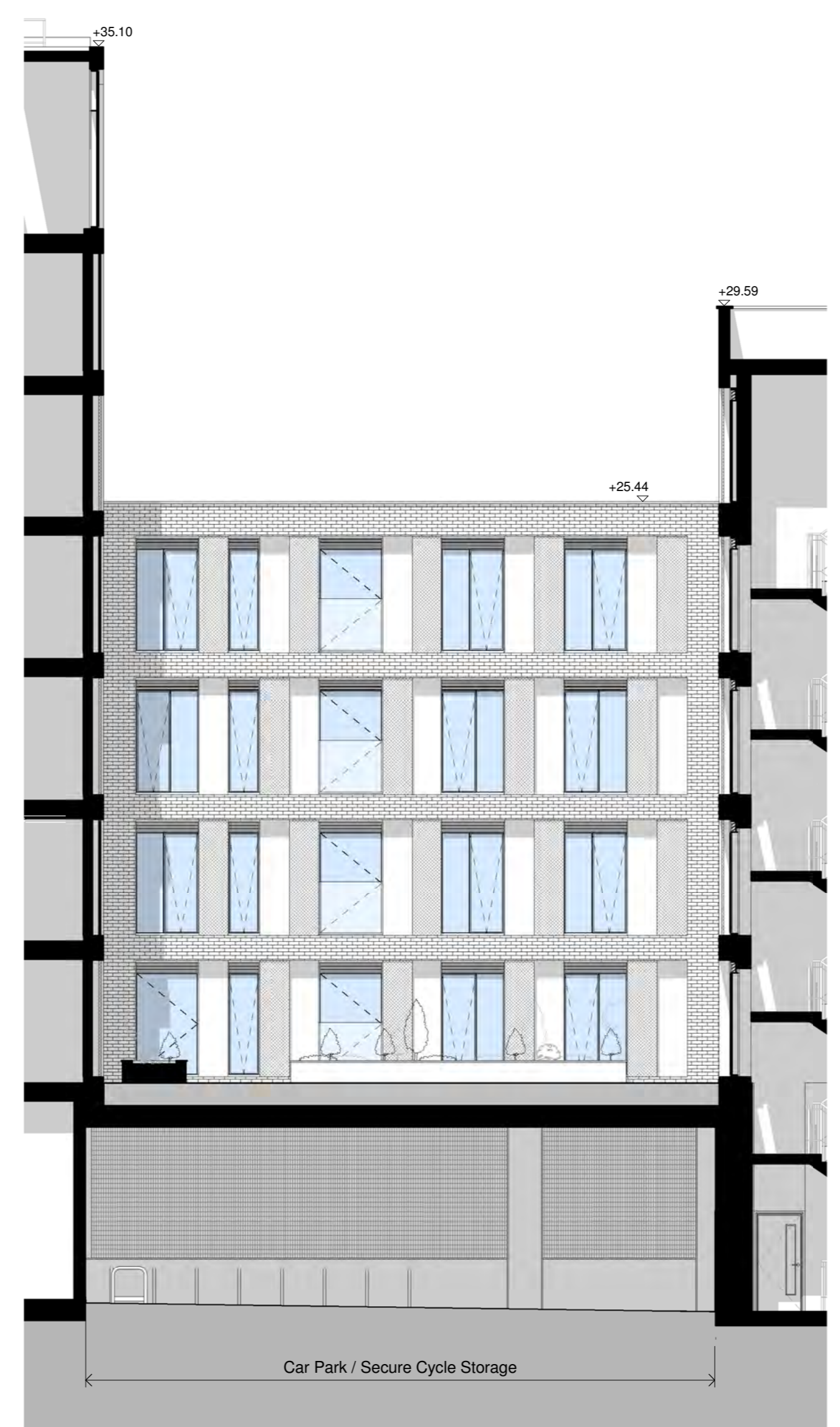
Existing building materials are illustrative only and are based on Sumo Survey Services drawing 140776 02 A

+0.00 Proposed Level

+0.00 Existing Level



1 | BLOCK B - SOUTH ELEVATION
1:100



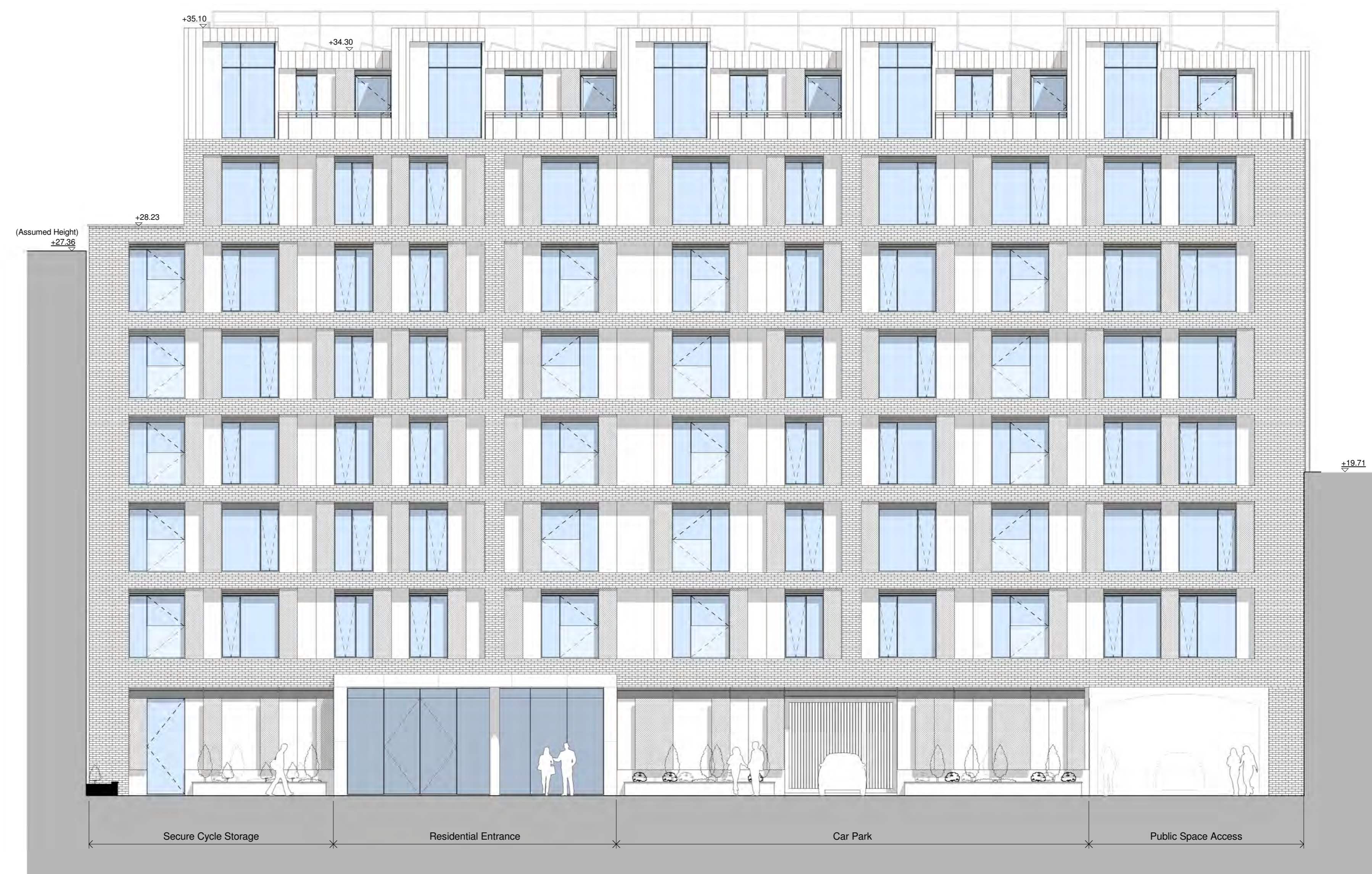
2 | BLOCK B - AMENITY SPACE 3 - SOUTH ELEVATION
1:100



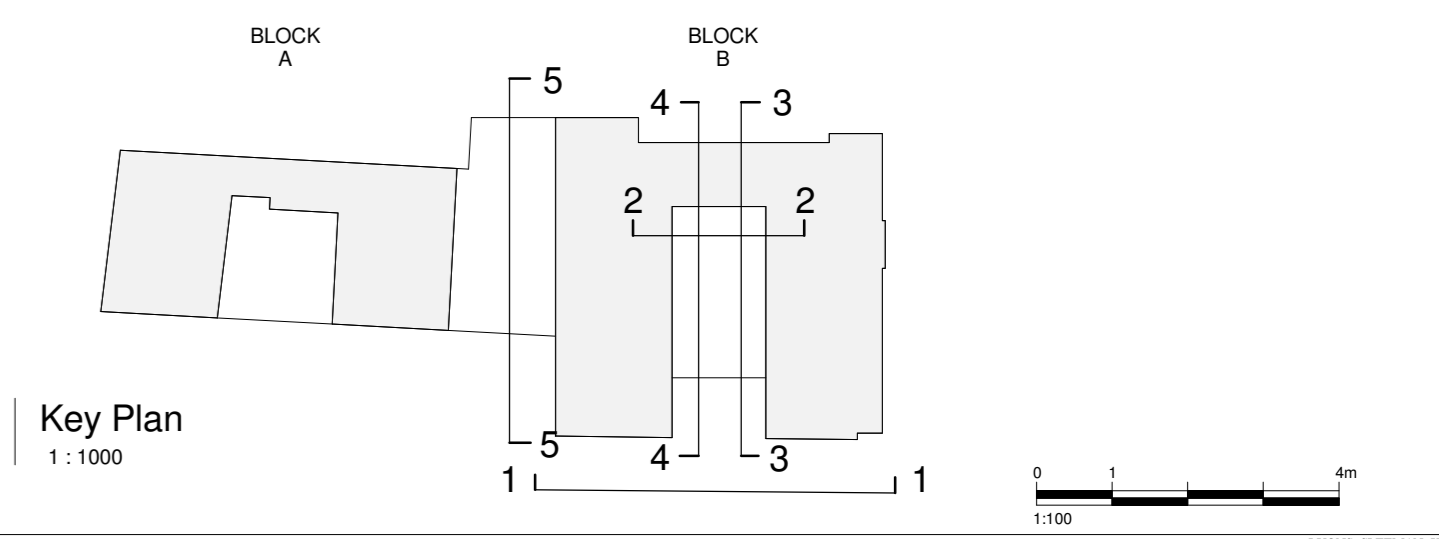
3 | BLOCK B - AMENITY SPACE 3 - WEST ELEVATION
1:100



4 | BLOCK B - AMENITY SPACE 3 - EAST ELEVATION
1:100



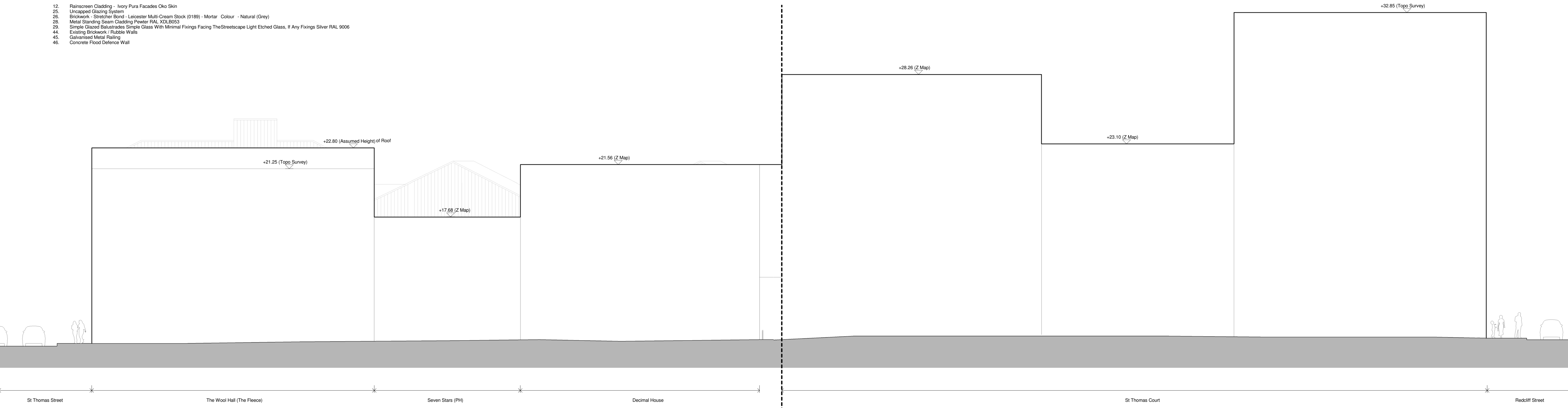
5 | BLOCK B - WEST ELEVATION
1:100



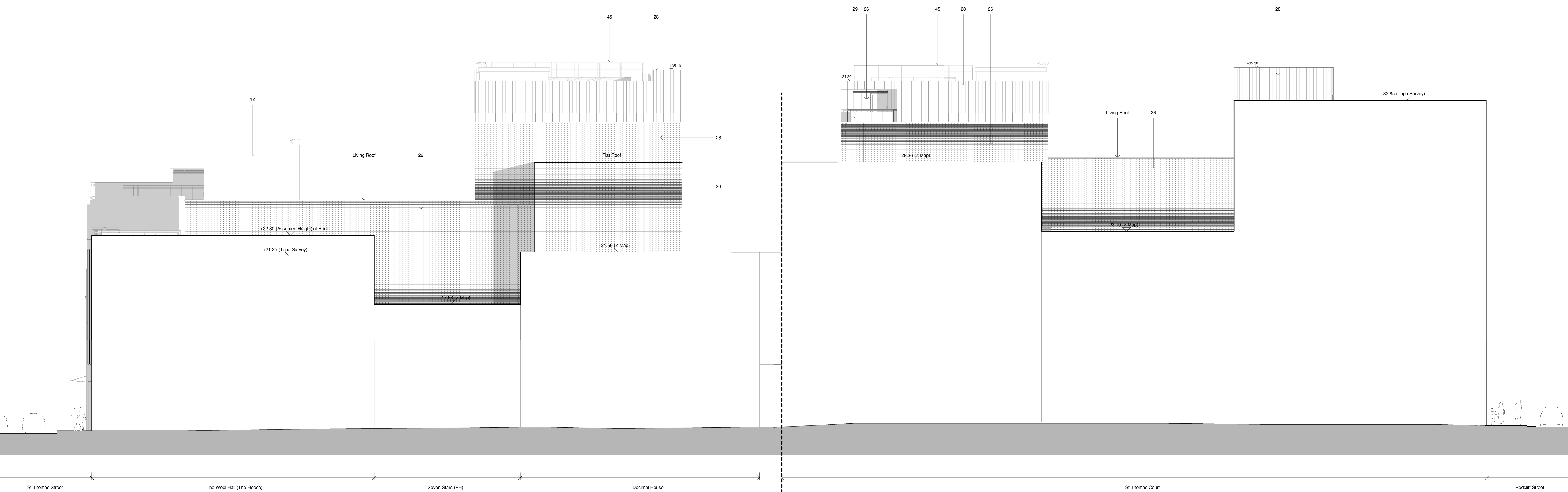
Job No	Rev	Scale	Status	Job
14/006 / P-303	B	As Indicated	PLANNING	SAMPSON COURT BRISTOL
Date	Author	Checked	Drawn	
12/09/2014	DH	AH	RW	PROPOSED BLOCK B INTERNAL ELEVATIONS

NOTE: OUTLINES OF EXISTING BUILDINGS SHOWN ARE INDICATIVE AND ARE SUBJECT TO A DETAILED SITE SURVEY. HEIGHTS OF BUILDINGS SHOWN HAVE BEEN TAKEN FROM TOPOGRAPHICAL SURVEY DATA WHERE POSSIBLE AND THEN BY Z MAP DATA IF THIS WAS NOT AVAILABLE.

- Proposed Materials**
- 12. Rainscreen Cladding - Ivory Pura Facades Oko Skin
 - 25. Uncapped Glazing System
 - 35. Brickwork - Stretcher Bond - Leicester Multi-Cream Stock (0189) - Mortar Colour - Natural (Grey)
 - 28. Metal Standing Seam Cladding Powder RAL XDLB053
 - 29. Simple Glazed Balustrades Simple Glass With Minimal Fixings Facing The Streetscape Light Etched Glass. If Any Fixings Silver RAL 9006
 - 44. Existing Brickwork / Rubble Walls
 - 45. Galvanised Metal Railing
 - 46. Concrete Flood Defence Wall



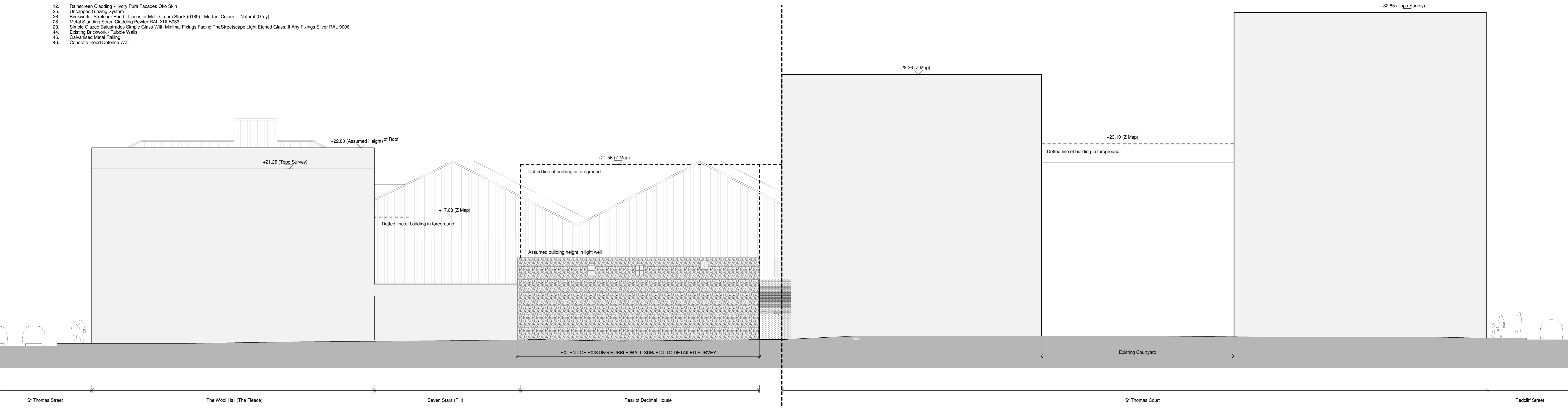
1 | EXISTING NORTH ELEVATION
1:100



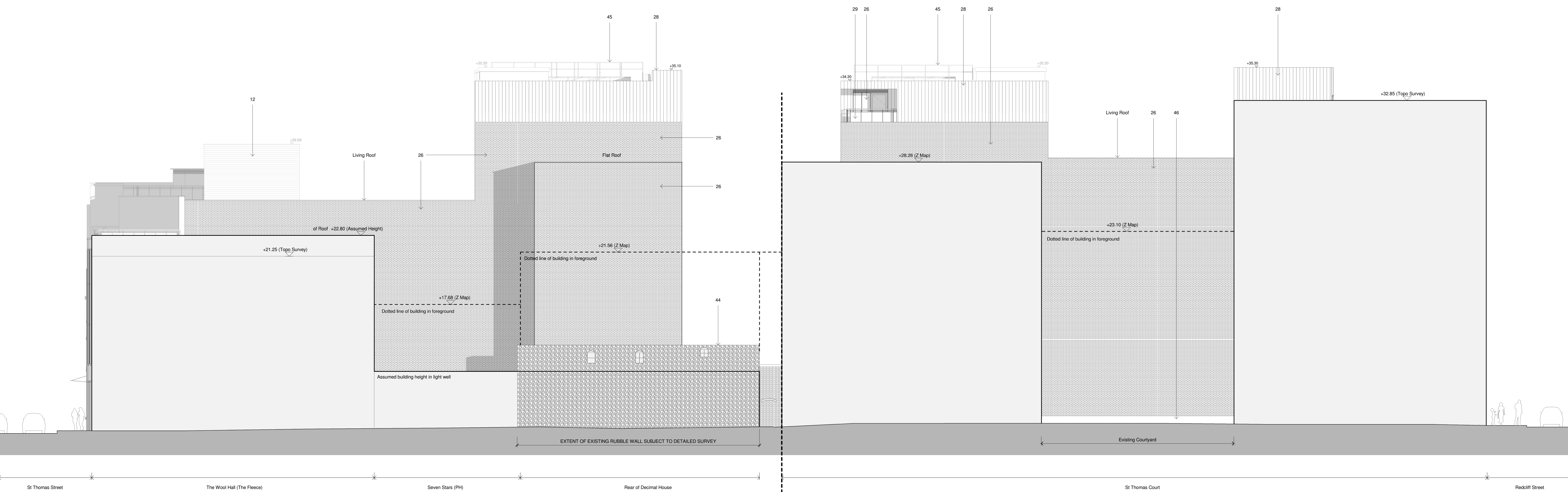
2 | PROPOSED NORTH ELEVATION
1:100

NOTE: OUTLINES OF EXISTING BUILDINGS SHOWN ARE INDICATIVE AND ARE SUBJECT TO A DETAILED SITE SURVEY. HEIGHTS OF BUILDINGS SHOWN HAVE BEEN TAKEN FROM TOPOGRAPHICAL SURVEY DATA WHERE POSSIBLE AND THEN BY Z MAP DATA IF THIS WAS NOT AVAILABLE.

- Proposed Materials**
- 12. Rainscreen Cladding - Ivory Pura Facades Oko Skin
 - 25. Unclipped Glazing System
 - 26. Brickwork - Stretcher Bond - Leicester Multi-Cream Stock (0189) - Mortar Colour - Natural (Grey)
 - 28. Metal Standing Seam Cladding Powder RAL XDLB053
 - 29. Simple Glazed Balustrades Simple Glass With Minimal Fixings Facing The Streetscape Light Etched Glass. If Any Fixings Silver RAL 9006
 - 44. Existing Brickwork / Rubble Walls
 - 45. Galvanised Metal Railing
 - 46. Concrete Flood Defence Wall



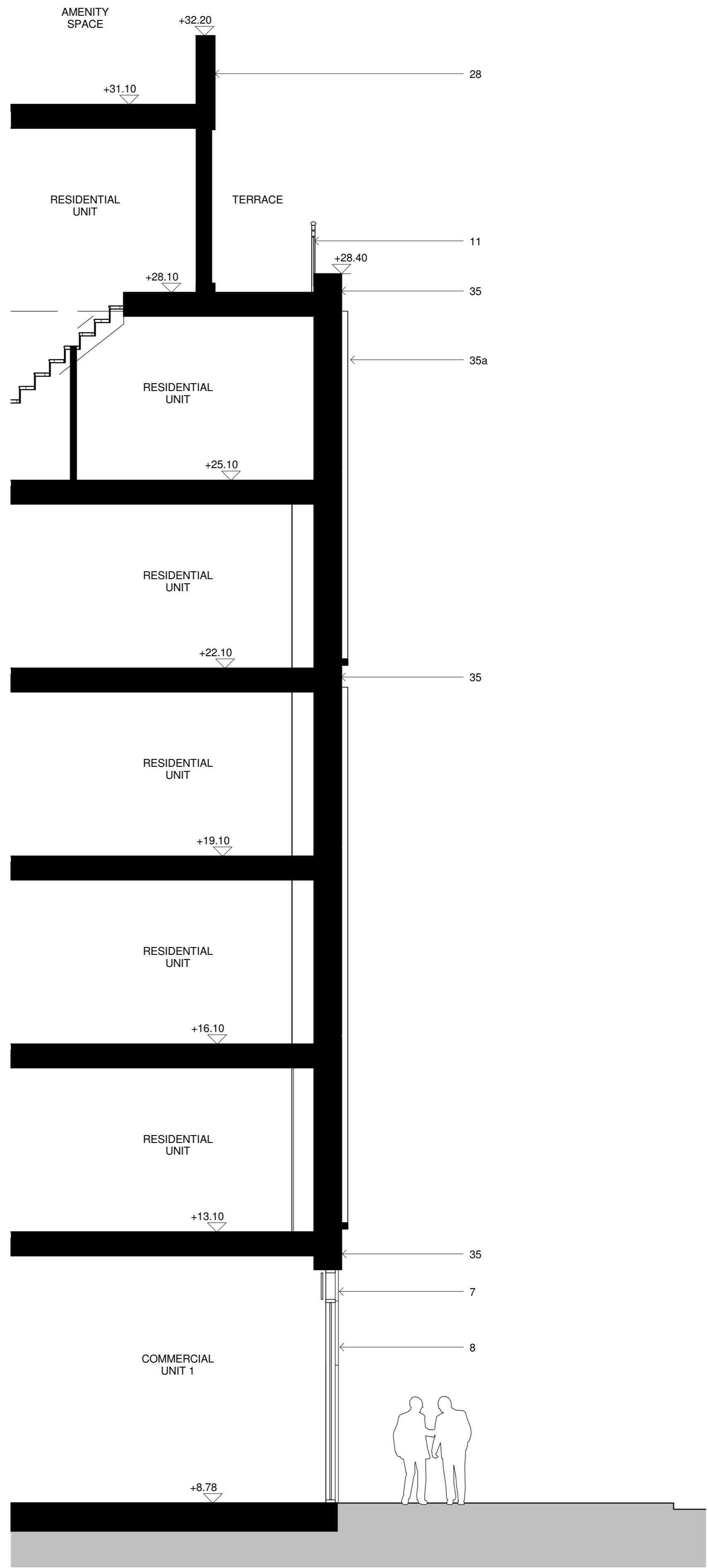
1 | EXISTING INTERNAL NORTH ELEVATION
 1:100



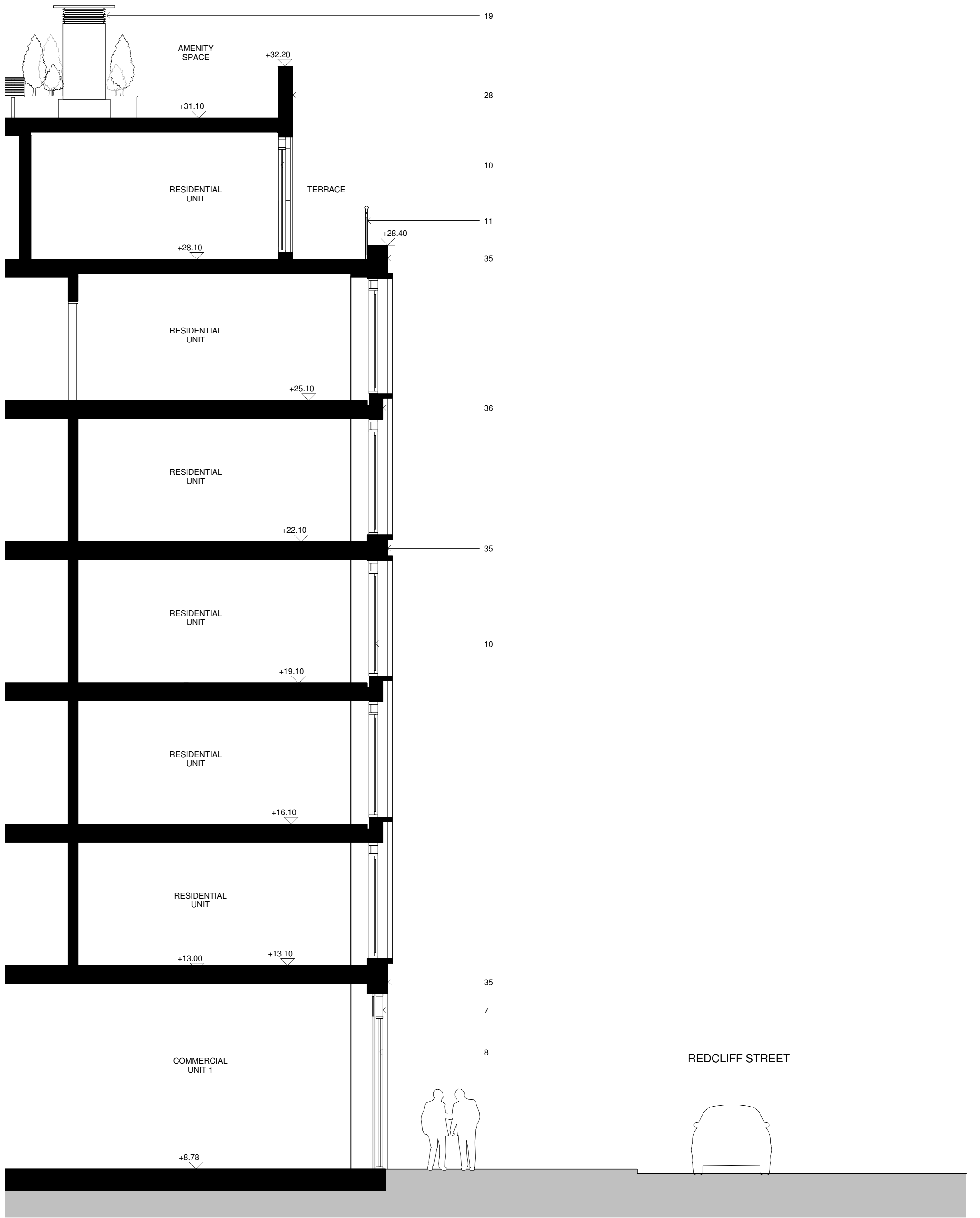
2 | PROPOSED INTERNAL NORTH ELEVATION
 1:100



1 PART REDCLIFF STREET ELEVATION
 1:50

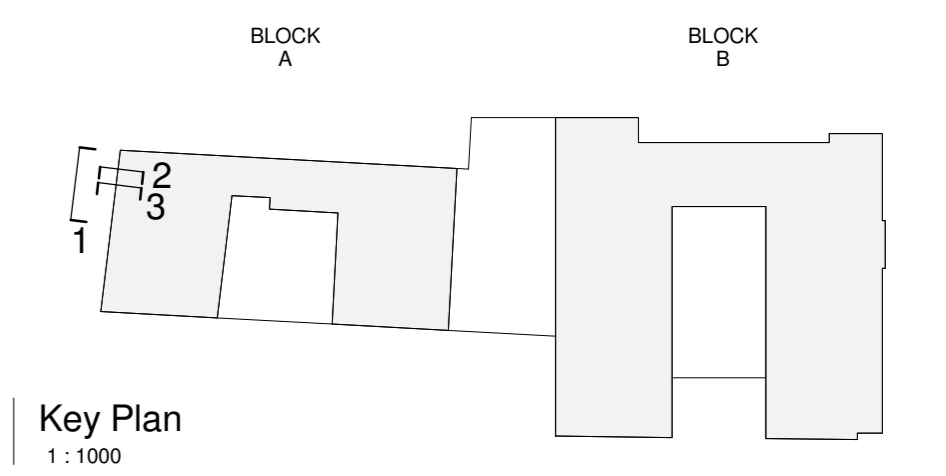


2 SECTION AA
 1:50



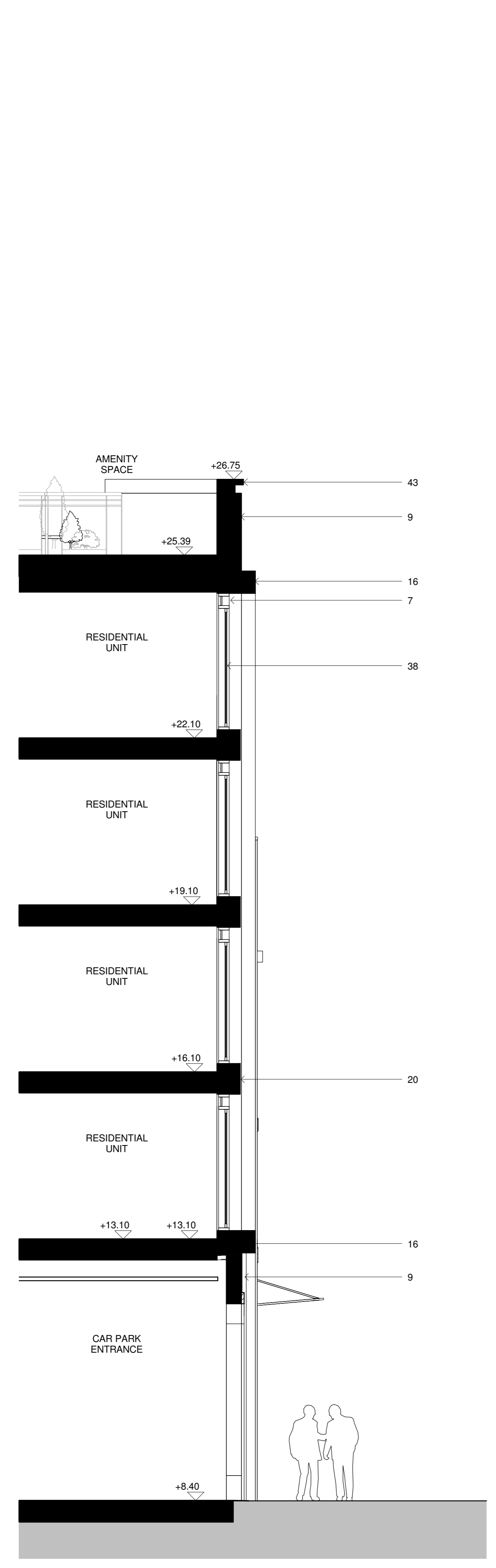
3 SECTION BB
 1:50

- Proposed Materials**
- Retained Facade - Existing Materials Refurbished To Required Regulatory Standards
 - New Timber Window Frames To Match Existing Style - Brown RAL 8070 Gloss Paint Finish
 - Simple Frameless Glass Canopy With Fitting Pattern
 - New Timber Door - Brown RAL 8070 Gloss Paint Finish
 - Steel Door To Bin Store - Pewter RAL - XDLB053 Or Gypsum RAL - XDKB018
 - Aluminum Curtain Wall System - Pewter RAL XDLB053
 - Metal Louvred Panel To Top Head Of Windows and doors - Colour To Match Frame RAL
 - Glazed Shop Fronts - Pewter RAL XDLB053 Or Gypsum RAL XDKB018
 - Bath Stone Effect GRC Panels - Trinity Bluff - Honed (Slight Texture), Large Format
 - Bath Stone Effect GRC Panels Copping Detail
 - Capped Aluminium Glazing System - Pewter RAL XDLB053
 - Simple Glazed Balustrades - Simple Glass with Minimal Fixings Facing The Streetscape Light Etched Glass, If Any Fixings Pewter RAL XDLB053
 - Rainscreen Cladding - Ivory Pura Facades Oko Skin
 - Insulated Metal Cladding Panel - PPC, Gypsum RAL XDKB018
 - Brickwork - Stretcher Bond - Red/Yellow Mix - Mortar Light Grey, Bucket Handle Profile
 - Aluminum Capping - PPC, Pewter RAL - XDLB053
 - Metal Detail Framing Feature - Aluminium PPC, Gypsum RAL XDKB018 - Exposed Joints Secret Fixed
 - Cast Aluminium RWP Round With Feature Hoppers - Black
 - Metal Railing Juliet Balcony - Aluminium PPC, Colour To Match Window Frame
 - Roof Top Vent Slacks - GRC (Silver Grey Finish) With Metal Louvers (Pewter RAL - XDLB053)
 - Slats GRC - Exposed fixing, 1800 x 147 Planks - Refined On Site Colours Terra And Mokka Brown Mixed
 - Expressed Beam Feature Aesthetic - Gypsum RAL XDKB018
 - Concrete Block - Fair Facet, Flint (Natural) - Stretcher Bond - Mortar Buck&Handle Profile
 - Exposed Concrete Column - Painted White (RAL 9010)
 - Feature Panel Perforated Sheet, Round Hole Staggered Pitch - Aluminium PPC, Silver RAL 9006
 - Unclipped Glazing System
 - Brickwork - Stretcher Bond - Leicester Multi-Cream Stock (0189) - Mortar Colour - Natural (Grey)
 - Automatic Security Gate - Pewter RAL XDLB053
 - Metal Standing Seam Cladding Pewter RAL XDLB053
 - Simple Glazed Balustrades Simple Glass With Minimal Fixings Facing The Streetscape Light Etched Glass, If Any Fixings Silver RAL 9006
 - Water Attenuation Tank Under Courtyard deck, Covered with Marshall's Priton Paving Slabs - Silver Grey And Charcoal - Gravel Edges
 - Internal Courtyard Door Surrounds - Vertical Pura Facade Oko Skin Strips - Colours Terra And Mokka Brown Mixed
 - Glazed Doors To Courtyards - PPC Aluminium, Silver RAL 9006
 - Insulated Metal Cladding Panel - PPC, Silver RAL 9006
 - Portland Stone Effect GRC Panels - Portland, Honed (Slight Texture), Large Format
 - Portland Stone Effect GRC Copping Detail
 - Insulated Metal Cladding Panel - PPC, Pewter RAL XDLB053
 - Terrace With Thermal Plastic Lining - Black Surface Finish, White Lining
 - Capped Aluminium Glazing System - PPC, Gypsum RAL XDKB018
 - Glazed Juliet Balconies Fixings To Match Window Frame RAL Colour
 - Glazed Doors Entrance - PPC Aluminium, Pewter RAL XDLB053
 - Fitted Glass Partition
 - Simple Glazed Balustrades Simple Glass with Minimal Fixings Facing The Streetscape Light Etched Glass, If Any Fixings Gypsum RAL XDKB018
 - Aluminum Capping PPO Finish To Top Of Walls, Gypsum RAL XDKB018
 - Existing Brickwork / Rubble Walls
 - Galvanised Metal Railing
 - Concrete Flood Defence Wall





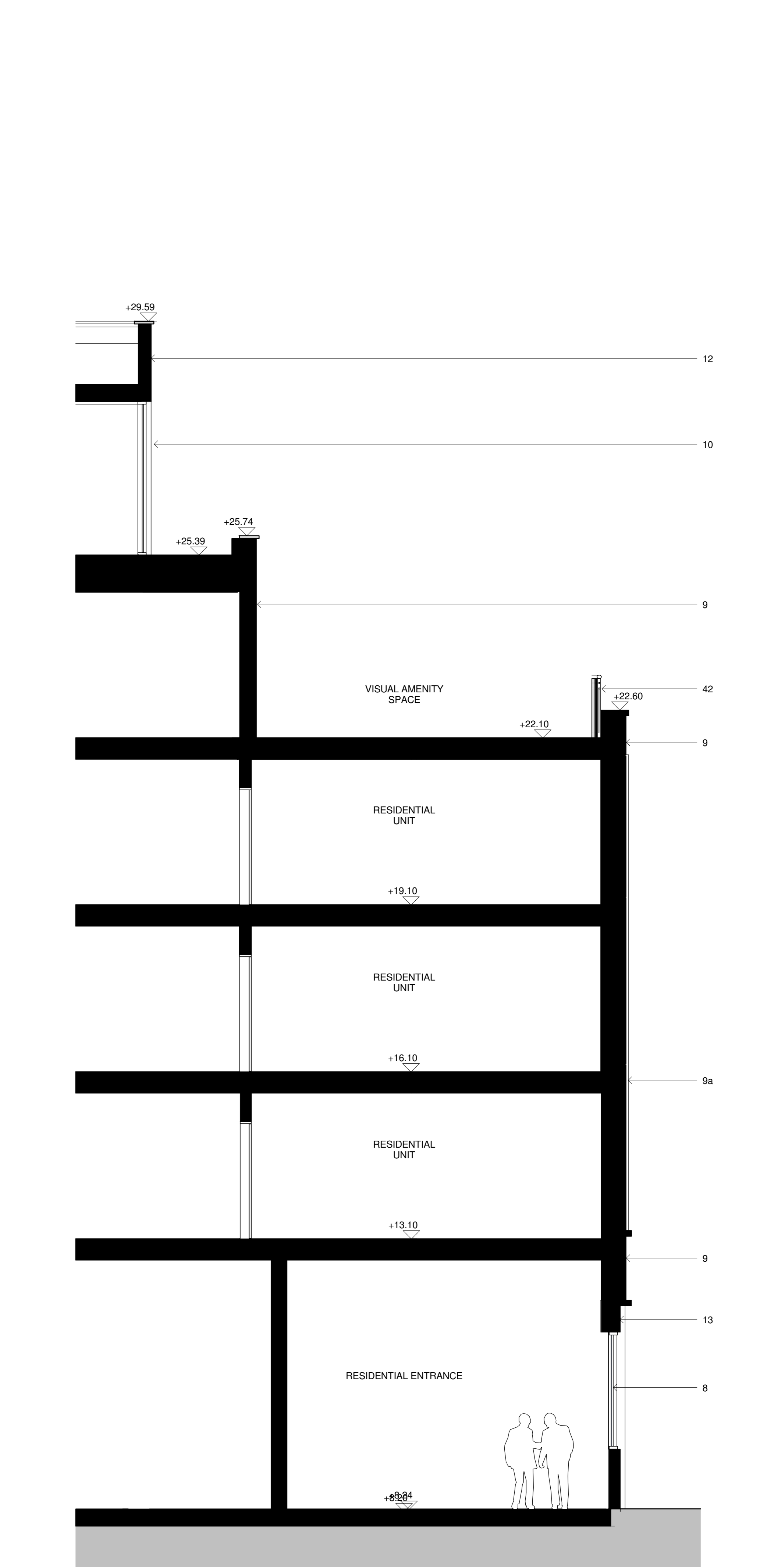
1 PART ST THOMAS STREET ELEVATION
 1:50



2 SECTION AA
 1:50

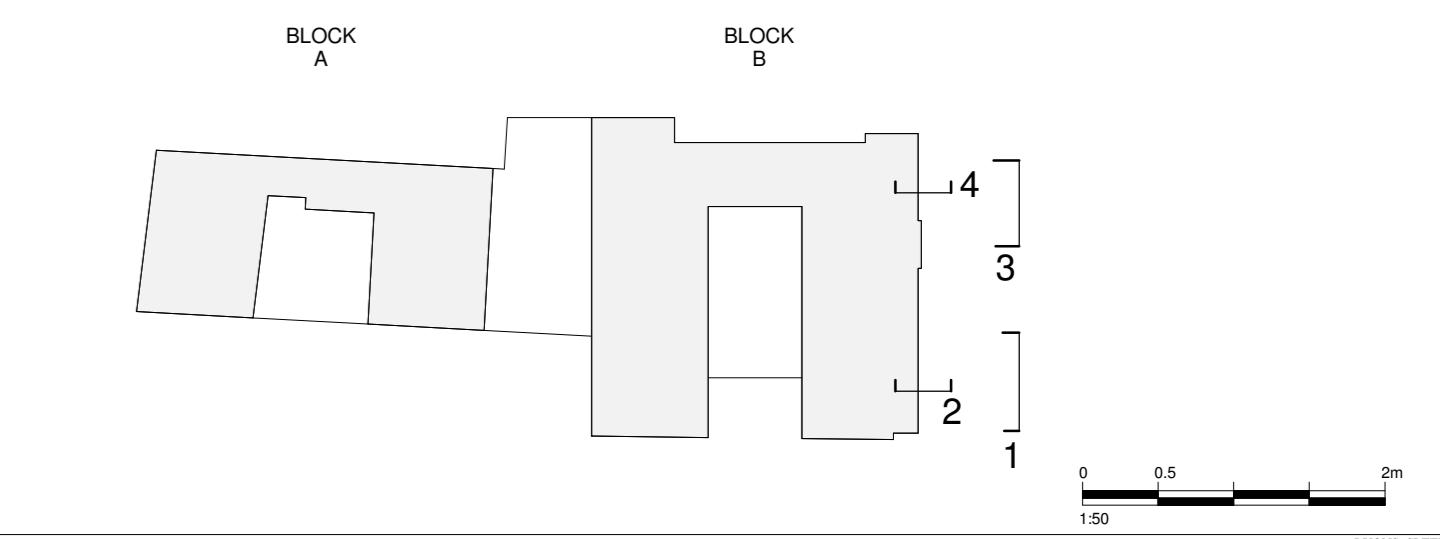


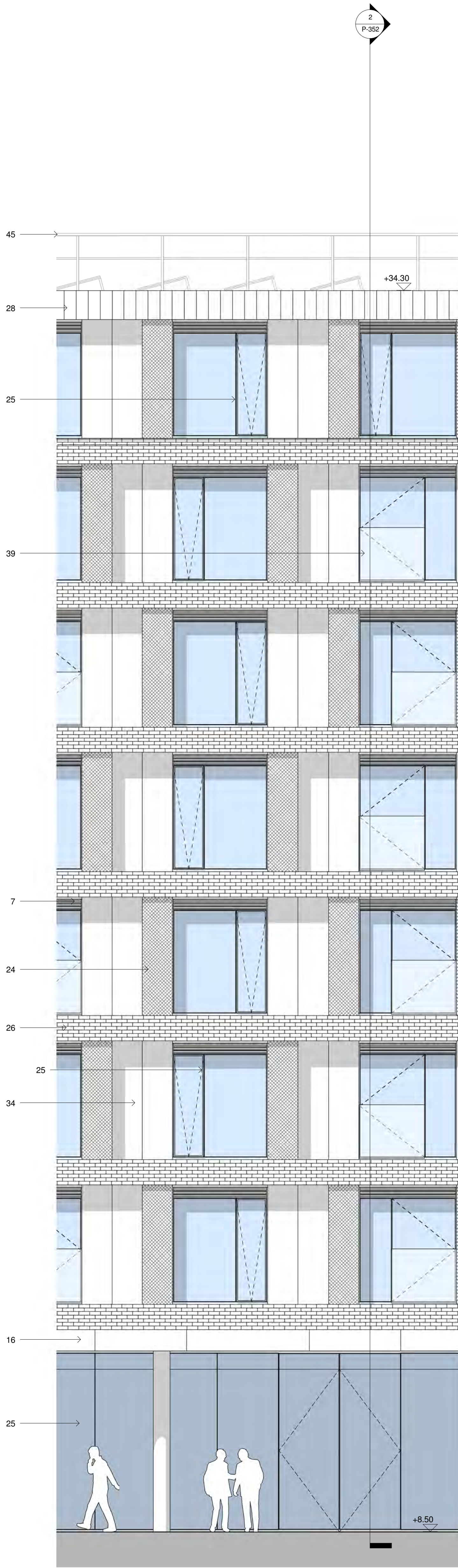
3 PART ST THOMAS STREET ELEVATION
 1:50



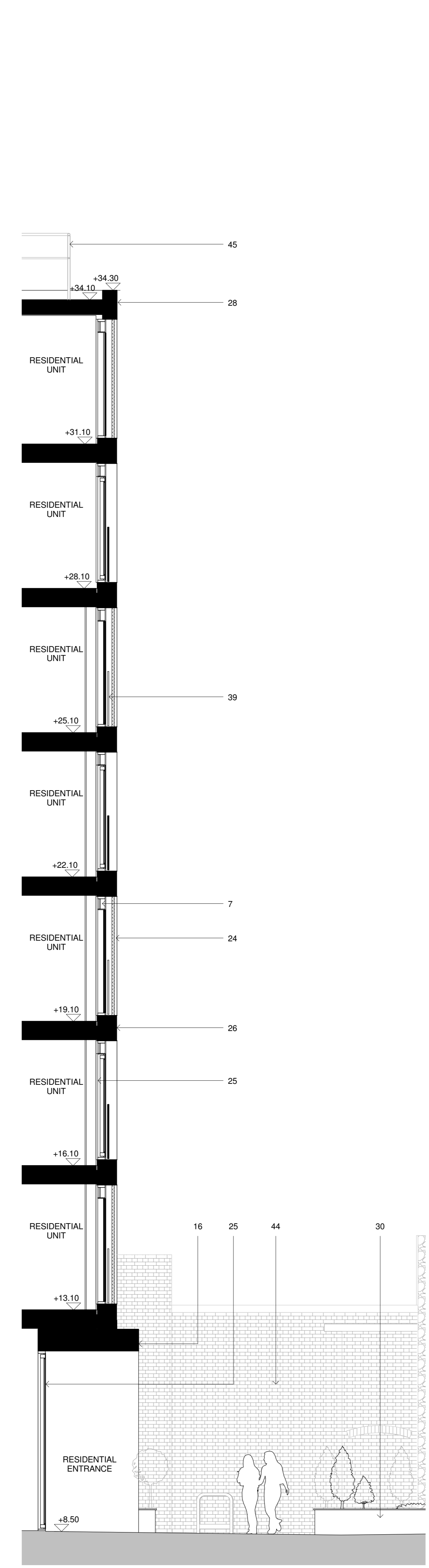
4 SECTION BB
 1:50

- Proposed Materials**
- Retained Facade - Existing Materials Refurbished To Required Regulatory Standards
 - New Timber Window Frames To Match Existing Style - Brown RAL 8070 Gloss Paint Finish
 - Simple Frameless Glass Canopy With Fitting Pattern
 - New Timber Door - Brown RAL 8070 Gloss Paint Finish
 - Steel Door To Bin Store - Pewter RAL - XDLB053 Or Gypsum RAL - XDKB018
 - Aluminum Curtain Wall System - Pewter RAL XDLB053
 - Metal Louver Panel To Top Head Of Windows and doors - Colour To Match Frame RAL
 - Glazed Shop Fronts - Pewter RAL XDLB053 Or Gypsum RAL XDKB018
 - Bath Stone Effect GRC Panels - Trinity Buff - Honed (Slight Texture), Large Format
 - Bath Stone Effect GRC Panels Coping Detail
 - Capped Aluminum Glazing System - Pewter RAL XDLB053
 - Simple Glazed Balustrades Simple Glass with Minimal Fixings Facing The Streetscape Light Etched Glass, If Any Fixings Pewter RAL XDLB053
 - Rainscreen Cladding - Ivory Pura Facades Oko Skin
 - Insulated Metal Cladding Panel - PPC, Gypsum RAL XDKB018
 - Brickwork - Stretcher Bond - Red/Yellow Mix - Mortar Light Grey, Bucket Handle Profile
 - Aluminum Cladding - PPC, Pewter RAL - XDLB053
 - Metal Detail Framing Feature - Aluminium PPC, Gypsum RAL XDKB018 - Exposed Joints Secret Fixed
 - Cast Aluminium RWP Round With Feature Hoppers - Black
 - Metal Railing Juliet Balcony - Aluminium PPC, Colour To Match Window Frame
 - Roof Top Vent Slacks - GRC (Silver Grey Finish) With Metal Louvers (Pewter RAL - XDLB053)
 - Slats GRC - Exposed fixing, 1800 x 147 Planks - Refined On Site Colours Terra And Mokka Brown Mixed Mixed
 - Expressed Beam Feature Aesthetic - Gypsum RAL XDKB018
 - Concrete Block - Fair Faced, Flint (Natural) - Stretcher Bond - Mortar Buck&Handle Profile
 - Exposed Concrete Column - Painted White (RAL 9010)
 - Feature Panel Perforated Sheet, Round Hole Staggered Pitch - Aluminium PPC, Silver RAL 9006
 - Unclipped Glazing System
 - Brickwork - Stretcher Bond - Leicester Multi-Cream Stock (0189) - Mortar Colour - Natural (Grey)
 - Automatic Security Gate - Pewter RAL XDLB053
 - Metal Standing Seam Cladding Pewter RAL XDLB053
 - Simple Glazed Balustrades Simple Glass With Minimal Fixings Facing The Streetscape Light Etched Glass, If Any Fixings Silver RAL 9006
 - Planters To Courtyards And Amenities Red Engineering Brick - Details To Be Confirmed
 - Water Attenuation Tank Under Courtyard deck. Covered with Marshall's Priton Paving Slabs - Silver Grey And Charcoal - Gravel Edges
 - Internal Courtyard Door Surrounds - Vertical Pura Facade Oko Skin Strips - Colours Terra And Mokka Brown Mixed
 - Glazed Doors To Courtyards - PPC Aluminium, Silver RAL 9006
 - Insulated Metal Cladding Panel - PPC, Silver RAL 9006
 - Portland Stone Effect GRC Panels - Portland, Honed (Slight Texture), Large Format
 - Portland Stone Effect GRC Coping Detail
 - Insulated Metal Cladding Panel - PPC, Pewter RAL XDLB053
 - Terraces With Thermal Plastic Lining - Black Surface Finish, White Lining
 - Capped Aluminum Glazing System - PPC, Gypsum RAL XDKB018
 - Glazed Juliet Balconies Fixings To Match Window Frame RAL Colour
 - Glazed Doors Entrance - PPC Aluminium, Pewter RAL XDLB053
 - Fritted Glass Partition
 - Simple Glazed Balustrades Simple Glass with Minimal Fixings Facing The Streetscape Light Etched Glass, If Any Fixings Gypsum RAL XDKB018
 - Aluminum Cladding PPO Finish To Top Of Walls. Gypsum RAL XDKB018
 - Existing Brickwork / Rubble Walls
 - Galvanised Metal Railing
 - Concrete Flood Defence Wall

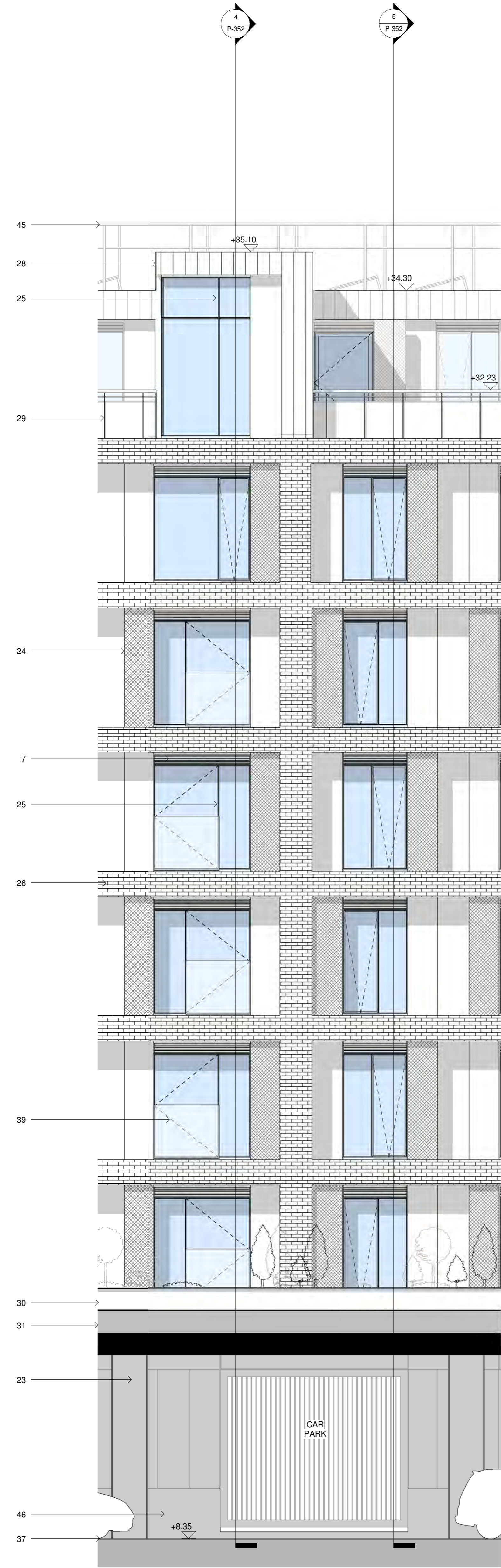




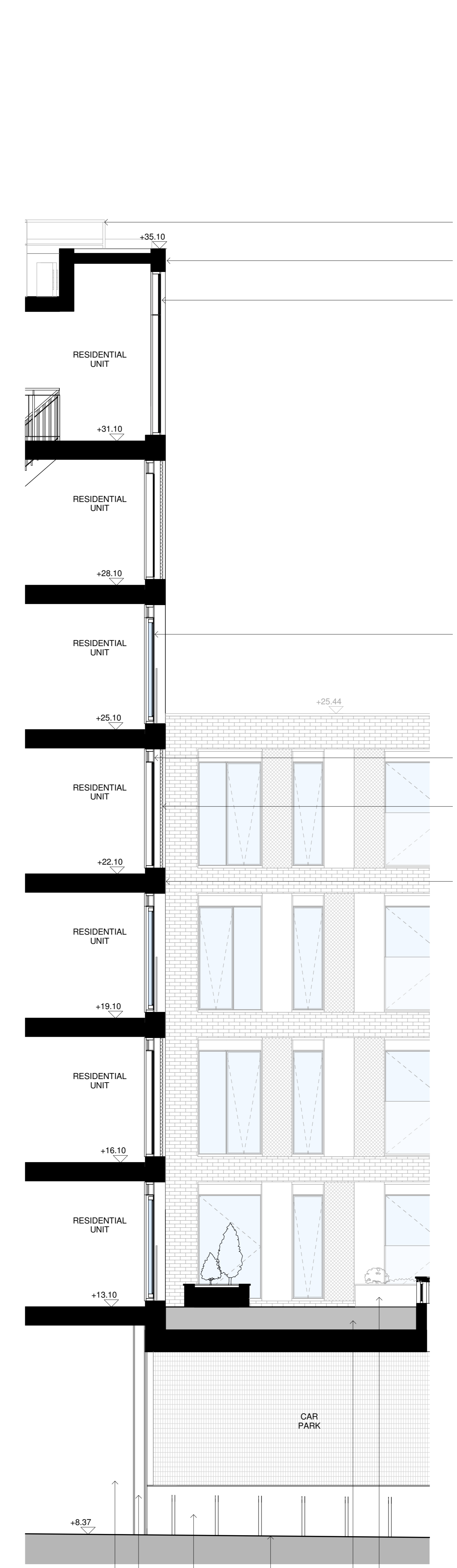
1 PART INTERNAL ELEVATION
1:50



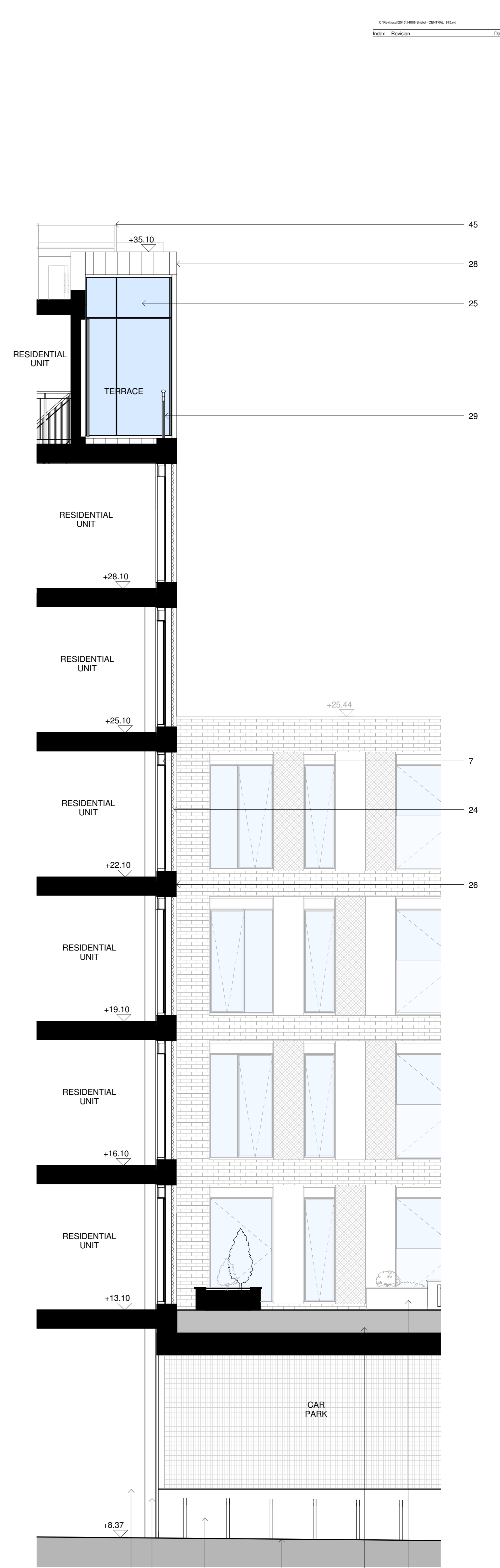
2 SECTION AA
1:50



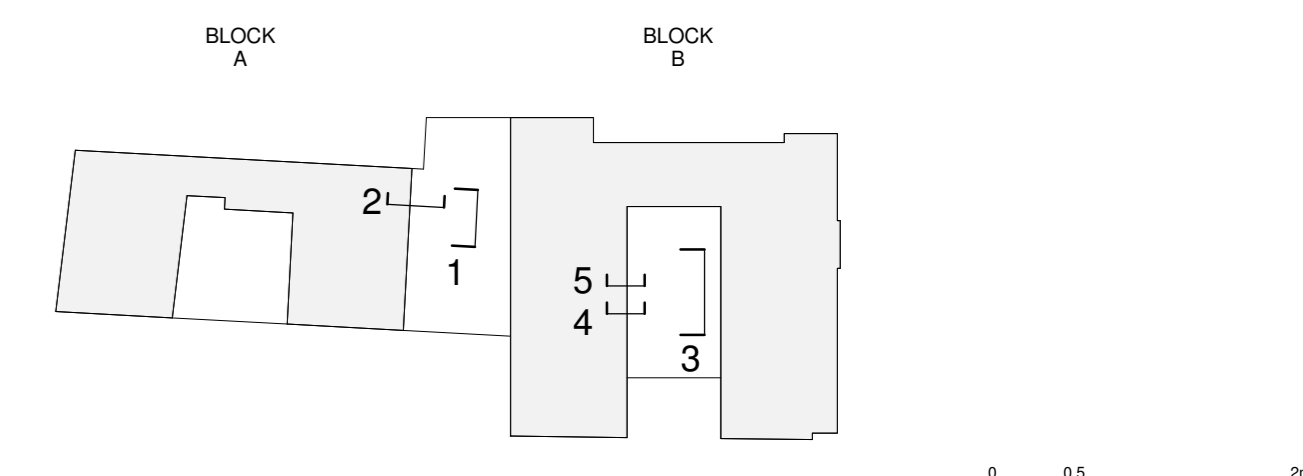
3 PART INTERNAL ELEVATION
1:50



4 SECTION BB
1:50



5 SECTION CC
1:50



- Proposed Materials**
- Retained Facade - Existing Materials Refurbished To Required Regulatory Standards
 - New Timber Window Frames To Match Existing Style - Brown RAL 8070 Gloss Paint Finish
 - Simple Frameless Glass Canopy With Fitting Pattern
 - New Timber Door - Brown RAL 8070 Gloss Paint Finish
 - Steel Door To Bin Store - Pewter RAL - XDLB053 Or Gypsum RAL - XDKB018
 - Aluminum Curtain Wall System - Pewter RAL XDLB053
 - Metal Louver Panel To Top Head Of Windows and doors - Colour To Match Frame RAL
 - Glazed Shop Fronts - Pewter RAL XDLB053 Or Gypsum RAL XDKB018
 - Bath Stone Effect GRC Panels - Trinity Buff - Honed (Slight Texture), Large Format
 - Bath Stone Effect GRC Panels Coping Detail
 - Capped Aluminum Glazing System - Pewter RAL XDLB053
 - Simple Glazed Balustrades - Simple Glass with Minimal Fixings Facing The Streetscape Light Etched Glass, If Any Fixings Pewter RAL XDLB053
 - Rainscreen Cladding - Ivory Pure Facades Oko Skin
 - Insulated Metal Cladding Panel - PPC, Gypsum RAL XDKB018
 - Brickwork - Stretcher Bond - Red/Yellow Mix - Mortar Light Grey, Bucket Handle Profile
 - Aluminum Cladding - PPC, Pewter RAL - XDLB053
 - Metal Detail Framing Feature - Aluminium PPC, Gypsum RAL XDKB018 - Exposed Joints Secret Fixed
 - Cast Aluminium RWP Round With Feature Hoppers - Black
 - Metal Railing Juliet Balcony - Aluminium PPC, Colour To Match Window Frame
 - Roof Top Vent Slacks - GRC (Silver Grey Finish) With Metal Louvers (Pewter/RAL - XDLB053)
 - Slabs GRC - Exposed fixing, 1800 x 147 Planks - Refined On Site Colours Terra And Mocca Brown Mixed
 - Expressed Beam Feature Aesthetic - Gypsum RAL XDKB018
 - Concrete Block - Fair Faced, Flint (Natural) - Stretcher Bond - Mortar Buck&Handle Profile
 - Exposed Concrete Column - Painted White (RAL 9010)
 - Feature Panel Perforated Sheet, Round Hole Staggered Pitch - Aluminium PPC, Silver RAL 9006
 - Unclipped Glazing System
 - Brickwork - Stretcher Bond - Natural (Grey)
 - Automatic Security Gate - Pewter RAL XDLB053
 - Metal Standing Seam Cladding Pewter RAL XDLB053
 - Simple Glazed Balustrades Simple Glass With Minimal Fixings Facing The Streetscape Light Etched Glass, If Any Fixings Silver RAL 9006
 - Aluminum Cladding PPO Finish To Top Of Walls, Gypsum RAL XDKB018
 - Existing Brickwork / Rubble Walls
 - Galvanneal Metal Railing
 - Concrete Flood Defence Wall
 - Water Attenuation Tank Under Courtyard deck, Covered with Marshall's Priton Paving Slabs - Silver Grey And Charcoal - Gravel Edges
 - Internal Courtyard Door Surrounds - Vertical Pure Facades Oko Skin Strips - Colours Terra And Mocca Brown Mixed
 - Glazed Doors To Courtyards - PPC Aluminium, Silver RAL 9006
 - Insulated Metal Cladding Panel - PPC, Silver RAL 9006
 - Portland Stone Effect GRC Panels - Portland, Honed (Slight Texture), Large Format
 - Portland Stone Effect GRC Coping Detail
 - Insulated Metal Cladding Panel - PPC, Pewter RAL XDLB053
 - Terrace With Thermal Plastic Lining - Black Surface Finish, White Lining
 - Capped Aluminum Glazing System - PPC, Gypsum RAL XDKB018
 - Glazed Juliet Balconies Fittings To Match Window Frame RAL Colour
 - Glazed Doors Entrance - PPC Aluminium, Pewter RAL XDLB053
 - Fitted Glass Partition
 - Simple Glazed Balustrades Simple Glass with Minimal Fixings Facing The Streetscape Light Etched Glass, If Any Fixings Gypsum RAL XDKB018
 - Aluminum Cladding PPO Finish To Top Of Walls, Gypsum RAL XDKB018
 - Existing Brickwork / Rubble Walls
 - Galvanneal Metal Railing
 - Concrete Flood Defence Wall



LYONS+SLEEMAN+HOARE | Architects

Nero Brewery, Cricket Green, Hartley Wintney, Hants RG27 8QA
 Tel: 01252 844144 Fax: 01252 844800 Web: www.lsharch.co.uk

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 PURPOSES ONLY**

Job N Drg N
 14/006 / P-400
 Date
 12/09/2014

Rev Scale Status
 B NTS PLANNING
 Director Author Check
 DH DB RW

Job
 SAMPSON COURT, BRISTOL

Drawing
 ILLUSTRATIVE VIEW - PROPOSED REDCLIFF STREET FRONTAGE

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NOTE: All figures are approximate and have been measured and expressed in a manner as defined by the current edition of the RICS/ISVA Code of Measuring Practice. Figures relate to the current stage of the project and any development decisions to be made on the basis of this information should include due allowance for the increases and decreases inherent in the design and building processes. Reproduced from Ordnance Survey mapping under Licence Numbers AR152978 & 100020449

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LYONS+SLEEMAN+HOARE | Architects

Nero Brewery, Cricket Green, Hartley Wintney, Hants RG27 8QA
 Tel: 01252 844144 Fax: 01252 844800 Web: www.lsharch.co.uk

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Job N Drg N
 14/006 / P-401
 Date
 12/09/2014

Rev Scale Status
 B NTS PLANNING
 Director Author Check
 DH DB RW

Job
 SAMPSON COURT, BRISTOL

Drawing
 ILLUSTRATIVE VIEW - PROPOSED ST THOMAS STREET

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NOTE: All figures are approximate and have been measured and expressed in a manner as defined by the current edition of the RICS/ISVA Code of Measuring Practice. Figures relate to the current stage of the project and any development decisions to be made on the basis of this information should include due allowance for the increases and decreases inherent in the design and building processes. Reproduced from Ordnance Survey mapping under Licence Numbers AR152978 & 100020449

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LYONS+SLEEMAN+HOARE | Architects

Nero Brewery, Cricket Green, Hartley Wintney, Hants RG27 8QA
 Tel: 01252 844144 Fax: 01252 844800 Web: www.isharch.co.uk

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Job N Drg N
 14/006 / P-402
 Date
 12/09/2014

Rev Scale Status
 B NTS PLANNING
 Director Author Check
 DH DB RW

Job
 SAMPSON COURT, BRISTOL

Drawing
 ILLUSTRATIVE VIEW - PROPOSED INTERNAL COURTYARD

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